

**CITY OF LEMON GROVE
DOWNTOWN VILLAGE SPECIFIC PLAN**

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On-site signs for projects within the Specific Plan are subject to the regulations of the Sign Ordinance (Development Code Section 18.12). Please consult with City Staff prior to proceeding with sign concepts to determine the sign requirements for individual projects.

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CHAPTER I – INTRODUCTION

The Lemon Grove Downtown Village Specific Plan is primarily comprised of the Downtown Village and the Civic Center Concept areas as depicted in the City of Lemon Grove General Plan as Special Treatment Area 1 (STA 1). Bounded by North Avenue to the north, Olive Street to the west, Lincoln Street and Central Avenue to the south, and School Lane, Grove Street and Kempf Street to the east, land use is predominantly commercial and civic-oriented uses, with limited areas of single and multi-family housing, light industrial and professional buildings. (Figure 1)

This Specific Plan is intended to stimulate several important changes to downtown. These changes will provide for vibrant pedestrian-friendly and transit-oriented neighborhoods in central Lemon Grove. By increasing building heights and permitting a variety of residential densities in the Downtown Village, additional redevelopment activity will be created. The future lowering of the Trolley track to eliminate the Lemon Grove Avenue, Broadway and Central Avenue at-grade crossings, and creation of a transit station that integrates both busses and the San Diego Trolley on Main Street near Pacific Avenue, will create better traffic flow and a more pedestrian-friendly downtown atmosphere.

It shall also be noted that this Specific Plan reflects a vision to be implemented over a 15-20 year period, and therefore, may be amended over time to reflect the City's most current vision for the area.

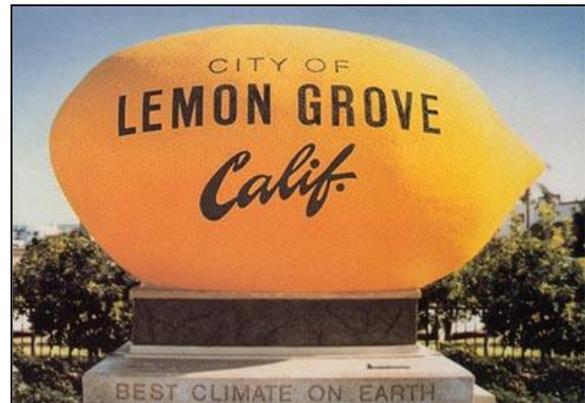
This chapter describes the Specific Plan project area and existing land uses, provides historic background information, addresses the purpose and intent of the Plan and explains the document's organization.

A. Purpose

The Lemon Grove Downtown Village Specific Plan provides a policy and regulatory bridge between the City of Lemon Grove General Plan and individual, project-level development. The Downtown Village Specific Plan includes

policies that will serve as guidance to decision-makers, and regulatory development standards and Design Guidelines intended to maintain Lemon Grove's downtown "village" character. It also includes a number of non-regulatory recommendations to other decision makers to address parking, signage and mobility.

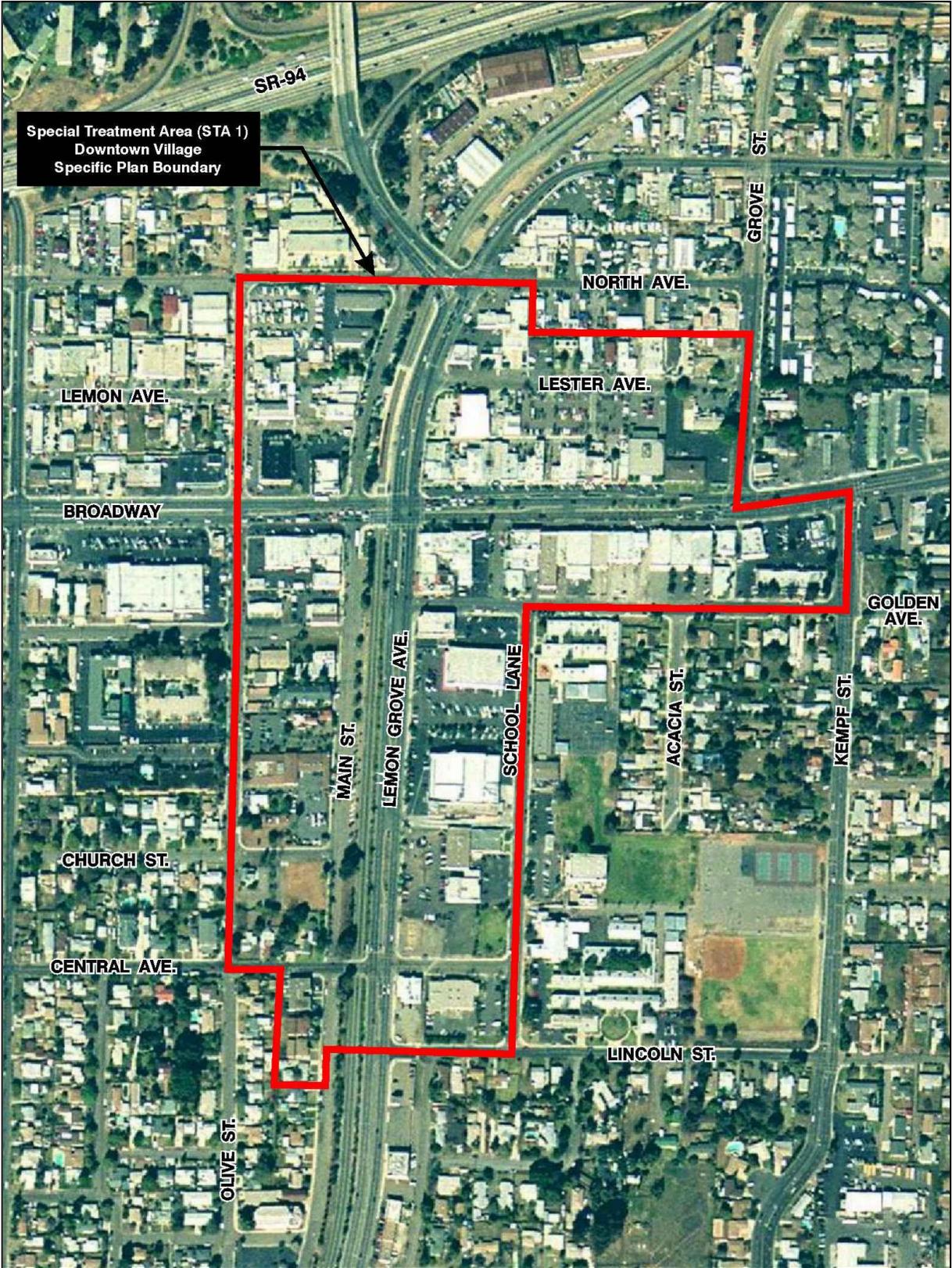
The City of Lemon Grove will use the Specific Plan as part of the project review and evaluation process. The Design Guidelines regulations, included in the Plan, inform property owners, developers, and design consultants as to what is required, expected and desired for downtown project design. Once adopted, the Downtown Village Specific Plan's regulations for zoning, development standards and Design Guidelines, will provide the legal development standards for the Specific Plan area. This Specific Plan shall not be considered an inflexible document. Rather, it has been developed to provide as much flexibility as allowed by State law.



According to the City of Lemon Grove General Plan,

The Downtown Village is planned for a lively mix of retail, office, condominium and apartment development supported by a variety of transportation options. In addition to supporting the village residents, shops and restaurants will provide recreational opportunities for Lemon Grove residents and others from adjacent communities. The residential population will in turn increase the viability of the village shops.





REGIONAL AERIAL MAP
Figure 1



B. Project Area

The City of Lemon Grove is located in the southwest portion of San Diego County. Lemon Grove is centrally located within the greater San Diego/Tijuana urban area, affording residents and businesses easy access to the region's population and employment centers.

Encompassing 3.75 square miles, the City boundaries are defined by the SR-94 freeway to the north, Sweetwater Road to the east, 69th Street and MacArthur Drive on the west, and residential streets to the south. The City of La Mesa lies just north of the City, across SR-94. To the east of the City is the unincorporated community of Spring Valley. Several City of San Diego communities wrap around the southern and western portions of the City. The SR-125 has recently been completed and extends along the City's eastern boundary.

The Downtown Village Specific Plan area covers approximately 58.3 gross acres and stretches from North Avenue south along Olive Street, Lemon Grove Avenue and Grove Street toward Lincoln Street and Central Avenue to the south. The east-west oriented streets including Lester Avenue, Broadway, Golden Avenue, Pacific Avenue, and Central Avenue are all within the boundaries of the project area, including several alleys that afford pedestrian and vehicular access to various land uses.

The existing downtown land uses have evolved since 1869. Current land uses include a mix of retail and office uses, however, some residential and light industrial land uses are found within the downtown village. The existing land uses can be described as a mixture of the following:

Residential- Residential uses are limited to several small apartment complexes.

Public/Institutional- These land uses include public uses and service facilities, such as government offices and facilities, schools, public utilities, post office, libraries, fire and law enforcement stations, social service facilities and churches.

Retail Commercial- These land uses are retail operations providing a broad range of goods and services, catering to both the local and regional customers. Includes shopping centers, department stores, grocery stores, professional services and other compatible retail businesses that are auto-oriented.

Parks/Recreation- Community and small parks, public recreation and a community center are included in this land use category.

San Diego Trolley- The Trolley runs north and south through the Downtown Village between Main Street and Lemon Grove Avenue. The current station is located adjacent to Main Street, north of Broadway.

C. Background



The history of Lemon Grove dates back to 1869 when Robert Allison purchased a portion of Rancho Mission San Diego. A rail line was extended from San Diego to Lemon Grove in 1890, and the production of citrus and berry crops boomed in the warm, temperate climate. The first subdivision was built in 1892 and between 15 and 20 structures were constructed. The center of town was established along the rail lines and included a small depot and the first store, which housed the post office and a school.

The City of Lemon Grove today is characterized by a well established land use pattern. The commercial and industrial areas are primarily located along Broadway, in the northern part of the City. The traditional downtown, (STA I), provides additional shopping opportunities primarily oriented to local residents and



workers. Commercial activity extends southward along Lemon Grove Avenue. The State Route 94 freeway and the San Diego Trolley Orange Line provide regional access.

The Lemon Grove monument – the famous 3,000-pound lemon – sits prominently in the Downtown Village, and recalls images of the community’s industrious spirit and agricultural roots. Moreover, a number of buildings from the old days still stand and provide a connection to the historic origins.

The City of Lemon Grove is committed to optimizing the Downtown Village’s redevelopment efforts through the development of this Specific Plan. Prior to this effort, the City established an Economic Development Task Force, prepared a Sign Ordinance, Redevelopment Plan, Assessment of Economic Development Opportunities, Downtown Storefront Improvement Guidelines and a Transit study. Most recently, in order to facilitate the potential for redevelopment of the Downtown Village, a Subcommittee was appointed to direct the preparation of this Specific Plan.

The challenges of this Downtown Village Specific Plan area are similar to redevelopment efforts for most downtowns and include:

- Providing convenient parking;
- Improving pedestrian, bicycle, auto and transit circulation;
- Enabling local business to compete with regional shopping centers;
- Maximizing the use of civic facilities; and
- Providing smart growth and transit-oriented development (TOD) opportunities in the areas near the Trolley station, including increasing residential density and commercial intensity.

These challenges are either not addressed by existing planning documents, or the existing provisions are written as guidelines and lack implementation mechanisms. In addition the

current Development Code does not adequately address smart growth or transit-oriented development principles.

In 2002, the City’s commitment to preserve and enhance Lemon Grove’s Downtown Village progressed with their decision to develop this Specific Plan.

D. Organization of the Specific Plan

This Specific Plan presents background information; policy direction; and zoning, land use, and Design Guidelines regulations for the Downtown Village area. The Specific Plan is organized into ten chapters, as described below:

Chapter 1: Introduction

This Chapter provides an overview of the Lemon Grove Downtown Village Specific Plan, including intent and purpose of the plan, some historic background information and authority of the document.

Chapter 2: The Vision

This Chapter provides the vision and policy foundation for the Specific Plan document. The Specific Plan Vision describes public outreach activities, planning factors, and Specific Plan goals and objectives related to the development and implementation of the Downtown Village Specific Plan.

Chapter 3: Planning Districts and Regulations

Chapter 3 provides permitted and discretionary uses, development standards, and the regulatory Design Guidelines for the Main Street Promenade, Village Core, Civic Center, and Marketplace Districts respectively.

Chapter 4: Mobility, Public Signage and Parking

This Chapter presents information on vehicular circulation, transit, bicycles, pedestrians, public signage and parking conditions within the



Specific Plan area. It also discusses future changes to public transit and traffic circulation.

Chapter 5: Infrastructure

This non-regulatory Chapter summarizes the public utilities, infrastructure and services that are present and planned for the Specific Plan area. Specific services that are addressed include potable water, sewer, storm water, power, drainage, and telecommunications.

Chapter 6: Implementation and Incentives

This Chapter presents a range of programs and actions that are recommended for the successful revitalization of Downtown Lemon Grove. Major strategy components are outlined, public and private responsibilities and actions are described, and phasing and funding options of projects/programs are identified.

Appendices

The appendices provide background material related to the Specific Plan update process including the Downtown Storefront Improvement Guidelines.

E Relationship of the Specific Plan to the Municipal Code

The Lemon Grove Downtown Village Specific Plan is established through the authority granted to the City of Lemon Grove by California Government Code, Title 7, Division 1, Chapter 3, Article 8, Sections 65450 through 65457 (specific plans).

As expressed in California law, Specific Plans may be adopted either by ordinance or by resolution. This allows jurisdictions to choose whether their specific plans will be policy driven (adopted by resolution) or regulatory in nature (adopted by ordinance). This Downtown Village Specific Plan is a regulatory document that will be adopted by ordinance.

As set forth in the Government Code, Section 65451, a Specific Plan must contain text and diagrams to address the following:

- The distribution, location and extent of the uses of land within the area covered by the plan.
- The proposed distribution, location, extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy and other essential facilities proposed to be located within the area covered by the Specific Plan and needed to support the land uses described in the plan.
- Development Standards and Design Guidelines by which development will proceed, and standards for the conservation, development and utilization of natural resources, where applicable.
- A program of specific measures, including regulations, programs, public works projects, and financing, necessary for implementation.
- The Specific Plan includes a statement of the relationship of the Specific Plan to the General Plan.

Development Standards and Design Guidelines contained in this Specific Plan will supplement or replace those of the City of Lemon Grove's Municipal Code. Whenever the regulations contained herein conflict with the provisions of the Municipal Code, the regulations of the Specific Plan shall apply. Any development standards or land use regulations not specifically covered herein shall be subject to the regulations, standards, and specifications of the City of Lemon Grove Municipal Code.

Definitions

Words, phrases and terms not specifically defined herein shall have the same definition as provided in the Municipal Code.

When used in this Specific Plan, the term "Director" shall mean the Community Development Director or an appointed representative.



When used in this Specific Plan, the term “Department” shall mean the Community Development Department.

When used in this Specific Plan, the term “Commission” shall mean the City of Lemon Grove Planning Commission.

Interpretations

The Director shall have the responsibility to interpret the provisions of this Specific Plan. Any decision of the Director interpreting this Specific Plan may be appealed to the Planning Commission. All such interpretations shall be in written form and shall be permanently maintained. Any person may appeal a decision of the Planning Commission to the City Council.

Amendments

This Specific Plan, including the Land Use Map, may be amended as allowed by Government Code. Each amendment shall include all sections or portions of the Specific Plan that are affected by the change. An amendment may be initiated by the City Council, Planning Commission, or private property owner. Any amendment requested by a property owner may be subject to the fee schedule adopted by the City Council.

Severability

If any regulation, condition, program or portion thereof of this Specific Plan is held invalid or unconstitutional by any court of the competent jurisdiction, the portion shall be deemed a separate, distinct and independent provision and the invalidity of such provision shall not affect the validity of the remaining portions.

Discretionary Use Permits

Minor Use Permit

These provisions prescribe the procedures for the accommodation of uses with site or design requirements, operating characteristics, or potential adverse effects on surrounding uses, which have been determined to be of such nature

and intensity as to not require the level of review required for those activities governed by the Conditional Use Permit process.

The uses, subject to these regulations, may or may not be compatible with other land uses in the vicinity depending upon the circumstances of the individual case. The Minor Use Permit allows such other uses to be reviewed and provides for conditional approvals or disapprovals to assure that any area will assume or retain the characteristics intended by the General Plan and this Specific Plan. Minor Use Permit procedures are outlined in section 17.28.052 of the Municipal Code.

Conditional Use Permit

These provisions prescribe the procedures for the accommodation of uses with special site or design requirements, operating characteristics, or potential adverse effects on surrounding uses, and shall apply to all projects for which a Conditional Use Permit is required.

Each land use has its principally permitted uses, but other uses may or may not be compatible with their surroundings depending upon the circumstances of the individual case. The Conditional Use Permit allows such other uses to be reviewed and adequately controlled or prohibited to assure that any area will assume or retain the characteristics intended by this Specific Plan. Conditional Use Permit procedures are outlined in Section 17.28.050 of the Municipal Code.

Planned Development Permit

The Planned Development Permit (PDP) procedure is intended to provide a method whereby land may be designed and developed as a unit by taking advantage of modern site planning techniques; and to produce an environment of stable, desirable character which will be in harmony with the existing or potential development of the surrounding neighborhood; and to ensure development which meets standards of environmental quality, public health and safety, and efficient use of the City’s resources.



Municipal Code section 17.28.030 includes information regarding Planned Development Permits procedures and administration.

Nonconforming Uses and Structures

Municipal Code section 17.24.090 includes information regarding procedures and administration for Nonconforming Uses and Structures.

Review of Projects

Design review for each project proposed in the Specific Plan area is required. Design Guidelines presented in the Specific Plan are to be used by property owners, developers, architects, landscape architects, designers and others involved in the planning and design of a project in the Specific Plan area.

The Design Guidelines communicate the City's desired qualities and characteristics of development and are intended to promote quality design that is compatible with Lemon Grove's vision for its downtown.

The Design Guidelines will be applied during the City's Development Review Approval process. This procedure is meant to encourage the highest level of design quality while providing the flexibility necessary to encourage creativity in response to existing site conditions.

Typical project submittals shall include:

- site plan
- landscaping concept plan
- floor plans for each floor level
- architectural elevations
- materials and colors
- cross-sections with dimensions, street façade studies, or architectural elevations to show the site in relation to adjacent properties.

- other information as necessary to provide adequate analysis

The review of submitted materials will be completed by Staff. Staff will make findings whether the submittal conforms with the letter and intent of this Specific Plan. All such findings shall be in written form and shall be permanently maintained. These findings may be appealed to the Planning Commission and/or the City Council. All projects within the Specific Plan area are subject to review and approval in compliance with the provisions herein and the Municipal Code.

Design Guidelines in the Specific Plan

Design Guidelines for site planning, storefront design, buildings, landscape/ hardscape, signs and public spaces within the Downtown Village are provided. As stated in the Downtown Storefront Improvement Guidelines, "the overall Design effort shall be to create a logical sense of order, distinct identity, and visual continuity throughout the Downtown Village District."

The purpose of the Design Guidelines is to protect the special identity of the downtown.

The Guidelines are intended to:

- Improve the quality of physical changes
- Protect/enhance the architectural character
- Act as a basis for objective decision making
- Increase public awareness of architectural quality, and
- Prevent incompatible new construction

The Design Guidelines acknowledge the built environment and existing development patterns within Lemon Grove's downtown. They are meant to promote the positive design characteristics that currently exist in the downtown and to enhance and improve the existing architectural and open space features.



These Guidelines are less quantitative than the mandatory development standards and may be interpreted with some flexibility in the application to specific projects. The Design Guidelines complement the mandatory development standards contained in this Specific Plan by providing good examples of appropriate design solutions and by providing design interpretations of the various mandatory regulations.

Interpretation

Guidelines that employ the word “shall” are intended to be applied as stated. An alternative measure may be considered, however, if it meets or exceeds the intent of the Guidelines.

Applicability

The provisions of this section are applicable to all development within the Lemon Grove Downtown Village Specific Plan area.

Organization

The Design Guidelines are organized into the following sections:

- a. Site Planning and Street Design: Includes guidelines for building, and parking location and design in relationship to the street (its street edge condition), service areas, and three-dimensional criteria.
- b. Storefront/Facade Design: Provides guidelines that promote quality design for the ground floor of new buildings and rehabilitation of existing buildings.
- c. Landscaping/Hardscape: Provides a basic framework that is to be used in conjunction with new development.
- d. Signage and Identity: Sets forth general and specific guidelines to enhance the image of commercial areas and improve the effectiveness of signage for businesses.
- e. Public Spaces: Includes guidelines associated with plazas, courtyards, paseos, landscaping, and public art within the Specific Plan areas.
- f. Parking: Sets forth guidelines for design and location of on-street and off-street parking spaces and lots.



CHAPTER II – THE VISION

The overall vision for the Lemon Grove Downtown Village Specific Plan is to create a vibrant and sustainable downtown. A mix of land uses ensuring a variety of residential options, shopping in a village atmosphere, and employment opportunities are planned. The emphasis for downtown is to provide inviting public places, transit-oriented development, and build on the unique character and community of the City of Lemon Grove.

This Chapter provides an overview of the public outreach activities, community involvement and policy framework that have influenced the development of the Lemon Grove Downtown Village Specific Plan. In addition, this chapter explains the planning factors, goals and objectives that form the foundation of the Specific Plan's development standards, design criteria, and provisions.

A. Public Outreach & Community Input



Preparation of the Specific Plan included a comprehensive public participation process. The public was involved to identify concerns, generate ideas, provide clarity and resolve issues. The following is a brief description of the community involvement and outreach efforts:

Community Workshops

Workshop #1 and Downtown Walking Tour – May 29, 2003. The first public workshop and Downtown Walking Tour was held on May 29, 2003 to provide community members with the opportunity to contribute input and learn about the process. The attendees were divided into five walking tour groups and then toured the entire Downtown area, from Central Avenue to

Lester along Lemon Grove Avenue, Broadway and Main Street. Basic concepts of redevelopment and examples of successful projects were shared with the group, and then they were asked to provide their observations, comments and recommendations from the walking tour.

Workshop #2 – November 20, 2003. The consultant team focused this meeting on development concepts based on input from the first workshop. These concepts included land use, urban design, and pedestrian/transit circulation. Following the presentation, the attendees were asked to share their comments and questions regarding the development concepts. The main issues addressed were separate districts for downtown Lemon Grove, increased density, retail/manufacturing land use, additional parking, and circulation changes.

Technical Advisory Committee Meetings

A Specific Plan Subcommittee, consisting of two members from the City Council, one Planning Commissioner, and two representatives from the community, contributed their unique backgrounds and perspectives toward creation of the Lemon Grove Downtown Village Specific Plan. Over the course of several months, the consultants met numerous times with this group to provide information, discuss options and receive the Committee's input regarding goals and issues for the Specific Plan. The direction provided by the Committee was used to develop the Draft Specific Plan. The Committee also met several times independently without the consultants.

B. Specific Plan Goals & Objectives

The Lemon Grove Downtown Village Specific Plan policy framework provides the primary guidance for the Specific Plan. All future development and redevelopment within the Specific Plan area shall be consistent with and take direction from the Goals and Objectives expressed in this Section.



The Policy Framework for the Lemon Grove Downtown Village Specific Plan is organized as follows:

- **Specific Plan Planning Factors:** Identifies the existing opportunities and constraints that influence and contribute to the creation of the Specific Plan
- **Specific Plan Goals:** Lists the broad goals that guide future development and redevelopment in the Specific Plan Area.
- **Specific Plan Objectives:** Outlines more explicit policy statements to implement the Specific Plan's Goals.

Specific Plan Planning Factors

The planning factors addressed in this Specific Plan were derived from a list of inadequacies in the Downtown Village. These planning factors are based upon community input, market analysis, mobility and parking studies, and observations by the subcommittee and planning consultant team. The planning factors are listed below and grouped into the following categories:

- Policy
- Land Use & Urban Design
- Parking
- Market/Economic
- Transportation/Circulation

Policy Factors

- **Community-Based Plan:** The Specific Plan area, a requirement of the Lemon Grove General Plan, includes a large, vibrant commercial area referred to as “downtown” by residents, business and property owners. Each of the involved stakeholders has ideas about how these different areas shall be preserved or how they shall change. A community that directs its future can keep and

enhance what is valued, while encouraging and allowing changes that contribute to the community's vision.

- **Predictable/Understandable Criteria and Standards:** The City's existing Downtown Storefront Improvement Guidelines (City of Lemon Grove May 2001) are well-written and cite positive examples of design, but are non-specific and not regulatory. Developers' and designers' interpretation of such guidelines are sometimes different from the understanding of City staff or decision-makers.

Land Use & Urban Design Factors

- **Vacant Lots and Buildings:** Vacant buildings present unattractive gaps in Lemon Grove's downtown, influence the appearance of adjacent buildings and detract from the community's economic vitality.
- **Points of Community Pride:** The community has specific unique features with which it strongly identifies and wants to protect, such as the small town village feel, historic structures at the park, the Lemon Grove “Lemon” and the history of the community.
- **Central Civic Space:** The existing Civic Center is underutilized and a new library is planned.
- **Community Circulation Patterns and Signage:** It is difficult to navigate through the Downtown Village because of the lack of adequate signage and the circulation disruptions caused by the Trolley.

Parking Factors

- **Parking Requirements and Intensity of Development:** To satisfy parking requirements for new development, or for redevelopment, relatively large parking lots are required. The result is that smaller lots are difficult to redevelop with adequate on-site parking and may simply deteriorate over time. Lot consolidations to create projects



that could provide adequate on-site parking may be bigger than what the community wants.

- **Location of Public Parking:** The existing parking lots are difficult to locate and access.
- **Limited Vacant Land for Surface Parking Lots:** Land that could be used for surface parking in the specific plan area is very limited and competes with development potential.

Market/Economic Factors

- **Unique Retail:** Multiple areas of the Downtown currently offer distinct retail/service opportunities.
- **Commercial:** Commercial land uses are underutilized throughout the project area.
- **Residential:** The City of Lemon Grove has identified the need for more housing to support the vision of mixed-use and TOD.

Transportation/Circulation Factors

- **San Diego Trolley:** At-grade trolley lines create significant traffic and pedestrian circulation impacts.
- **Highway 94 Access:** Large volumes of traffic entering the community from Highway 94 create congestion at multiple intersections and several roadway segments within the Downtown area.
- **Integrated Modes of Transportation:** Multiple modes of transportation (trolley, bus, auto, bicycle and pedestrian) are available to the community but are poorly integrated.
- **Pedestrian Circulation:** Lemon Grove Avenue, the trolley tracks right-of-way, and Main Street hinder pedestrian circulation.

- **Pedestrian Linkages:** There is a lack of pedestrian linkages throughout the Downtown Village.
- **At-Grade Trolley Crossings:** The trolley station and at-grade street crossings at Central Avenue, Broadway and Lemon Grove Avenue adjacent to Highway 94 create vehicular and pedestrian congestion and safety concerns and divides the community.

Goals, Objectives and Implementation Strategies/Actions

The following goals and objectives for the Lemon Grove Downtown Village Specific Plan provide the foundation of the regulatory framework that is implemented through the land use plan, zoning regulations, development standards, and design criteria. These goals and objectives are intended to complement the evaluation of future proposals and design concepts to determine if they are supportive of the spirit and intent of this plan. The implementation strategies/actions support the goals and objectives. The Specific Plan will guide decision-makers to determine whether a project will contribute to the desired future of Lemon Grove.

Implementation Strategies and Actions are specific and measurable. They direct the City in specific ways to reach its goals. This includes not only the goals stated in the Specific Plan, but also the broader goals of the Lemon Grove General Plan.

Land Use

Goal (1): Create a vibrant downtown district that is visually attractive and compatible with the village atmosphere of the current downtown Lemon Grove.

Objective (1):

- Create a strong sense of identity for the Downtown Village through enhancement of streetscapes, architectural form, and site planning.



- Establish a comprehensive signage system of informational and directional signs to direct vehicular and other travelers to the Downtown districts and parking facilities.

Implementation Strategies/Actions (1):

- Create appropriate development standards and design criteria that promote the area.
- Develop and implement sidewalk and streetscape improvements in commercial areas.
- Create a landscape program that strengthens the individual identity of each district, enhances linkages between districts, and creates an inviting pedestrian atmosphere by introducing shade and pedestrian scale elements.
- Encourage signage that provides sufficient direction and identification.
- Encourage the use of art to enhance public areas.

Goal (2): Create a pedestrian friendly environment.

Objectives (2):

- Establish a land use pattern that provides a variety of uses.
- Encourage infill development to be redeveloped in a more visually distinctive and pedestrian-friendly way.
- Create separate development standards and design guidelines that create distinct districts within the Downtown Village.
- Ensure the implementation of the Specific Plan.

Implementation Strategies/Actions (2):

- Limit uses that do not contribute to the pedestrian environment.
- Divide the Downtown Village Specific Plan area into Districts with distinctive uses, development standards and design guidelines.

Goal (3): Encourage transit-oriented development to create comfortable, walkable neighborhoods centered around a safe and

attractive transit station serving multi-modal transportation. There are opportunities for changes in densities and implementation of smart growth development ideals to create a concentrated downtown core, and utilize transit-oriented development principles.

Objective (3):

- Create core uses that have a direct relationship to transit.

Implementation Strategies/Actions (3):

- Allow for high residential densities to be developed in close proximity to transit locations.
- Encourage safe, comfortable and convenient pedestrian crossing options.
- Encourage businesses surrounding the transit station serving local residents and commuters.

Parking

Goal (4): Obtain a parking supply that is balanced to support the viability of commercial district businesses while minimizing impacts on adjacent neighborhoods.

Objectives (4):

- Enhance the usability of transit stops (bus and trolley)
- Pursue parking agreements with commercial owners of underutilized parking lots or purchase land to provide public parking.

Implementation Strategies (4):

- Reduce the need for parking by promoting mass transit.
- Create an Integrated Transit Center in cooperation with the Metropolitan Transit System (MTS).
- Use signage to direct vehicles to appropriate convenient parking for their specific destination or activity.
- Create a program to collect fees from Downtown Village redevelopment projects that are unable to provide required parking on-site to provide additional public parking.



Economic Development

Goal (5): Create a long term, sustainable business environment for business and property owners.

Objectives (5):

- Encourage a unique retail environment.
- Enhance the pedestrian-oriented environment.
- Promote retention of existing businesses and recruitment of new businesses that are desired in Lemon Grove.
- Increase population within walking distance through greater residential densities downtown and allowing for residential development above existing storefronts.

Implementation Strategies (5):

- Develop funding mechanisms, where appropriate and feasible, to implement public improvements and business-improvement activities.
- Implement a well-coordinated maintenance program for sidewalks, streets, and landscaping.
- Develop a neighborhood that allows for people to satisfy all aspects of their lives including, work, residences, shopping, recreation, education and culture.
- Provide Wide-Area Wi-Fi to Downtown Business and Residential Districts.

Transportation/Circulation

Goal (6): Obtain a level of mobility (walking, bicycles, transit and auto) that furthers economic development, convenience, safety and choice for residents, visitors, and the business community.

Objectives (6):

- Enhance the usability of transit stops (bus and trolley).
- Accommodate the grade separation of trolley and streets.

Implementation Strategies (6):

- Pursue funding opportunities with MTS for creating grade-separated crossings for the Trolley at Central Avenue, Broadway and Lemon Grove Avenue.
- Reduce the number of auto trips and congestion through better management of parking supply, public transit improvements, and consideration of roadway improvements.
- When pedestrian/vehicular/cyclist conflicts require compromise, favor the pedestrian and cyclists.

Goal (7): Encourage sustainable development, meeting the needs of the present without compromising the ability of future generations to meet their own needs. In addition, promote green building design, which includes for example energy efficient design and renewable, nonpolluting materials.

Objectives (7):

- Create efficient infrastructure
- Use resources more efficiently
- Protect and preserve natural resources
- Improve economic development and building marketability
- Enhance occupant comfort, health and productivity

Implementation Strategies (7):

- Encourage project designs that incorporate a reduction of construction debris and building operating materials in the landfills
- Encourage project designs that achieve lower operating and maintenance costs while providing a comfortable environment for the occupants
- Continue to research and identify programs to the project proponents that provide incentives to achieve this goal.



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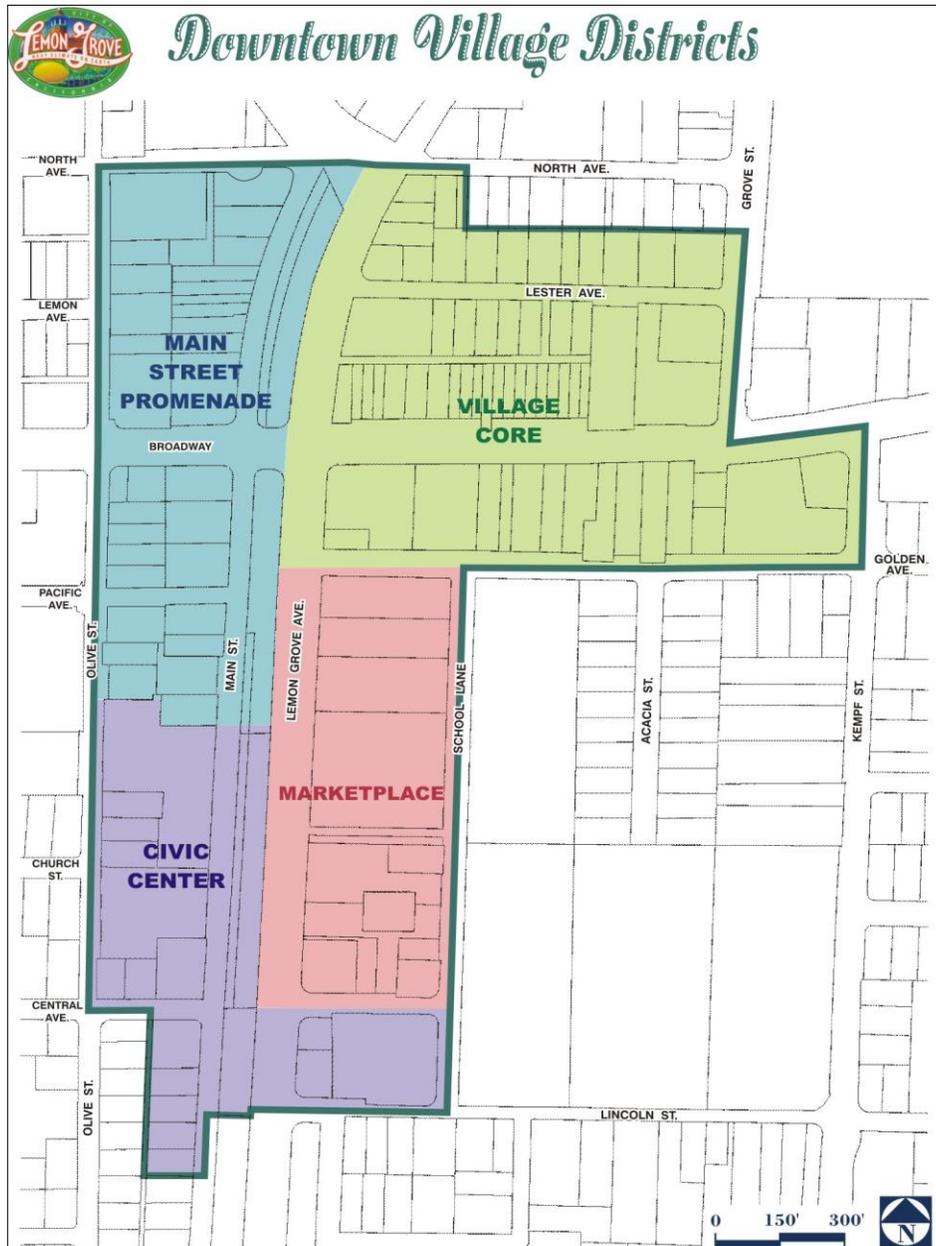
CHAPTER III – PLANNING DISTRICTS & REGULATIONS

A. Purpose

Upon adoption of the Lemon Grove Downtown Village Specific Plan, the development standards for the following Districts will supercede those set forth in the Municipal Code (Figure 2):

Downtown Village Districts

- Main Street Promenade District
- Village Core District
- Civic Center District
- Marketplace District



DOWNTOWN VILLAGE DISTRICTS
Figure 2



The following tables summarize net lot area, anticipated building areas and dwelling units in each of the four Districts created by the Downtown Village Specific Plan. Table 1 shows the net lot area within each zone for each District. Table 2 shows the anticipated building square footage by land use and the range of residential dwelling units within each District. The total amount of building area anticipated for each land use is shown in Table 3.

| Table 1. Lot Area by District/ Zone | | |
|---------------------------------------|------------------|--------------|
| District/Land Use Zone | Lot Area Sq. Ft. | Acres |
| Main Street Promenade | | |
| Transit Mixed Use (7) ¹ | 108,464 | 2.49 |
| Transit Mixed Use (5) ¹ | 78,844 | 1.81 |
| Transit Mixed Use (3) ¹ | 151,589 | 3.48 |
| Transportation (Trolley) | 91,476 | 2.10 |
| Subtotal | 430,373 | 9.88 |
| Village Core | | |
| Retail Manufacturing ² | 107,593 | 2.47 |
| Village Commercial (5) ³ | 121,968 | 2.80 |
| Village Commercial (3) ³ | 296,644 | 6.81 |
| Parking | 155,509 | 3.57 |
| Subtotal | 681,714 | 15.65 |
| Civic Center | | |
| Civic ⁴ | 189,172 | 4.34 |
| General Commercial ⁵ | 15,600 | 0.36 |
| Central Commercial ⁶ | 6,930 | 0.16 |
| Residential/Professional ⁷ | 28,460 | 0.65 |
| Residential Medium/High ⁸ | 29,475 | 0.68 |
| Transportation (Trolley) | 74,033 | 1.47 |
| Subtotal | 343,670 | 7.66 |
| Marketplace | | |
| Commercial ⁹ | 284,011 | 6.52 |
| Civic ⁴ | 22,216 | 0.51 |
| Subtotal | 306,227 | 7.03 |
| TOTAL | 1,761,984 | 40.22 |

Note: Transit Mixed Use and Village Commercial zones allow a mixture of Retail Commercial, Office and Residential land uses. See Table 2 for building area by land use and number of dwelling units.

- The three Transit Mixed Use zones assume 80% lot coverage, minimum residential density of 45, 35 and 25 dwelling units per acre and 7, 5 or 3 stories respectively.
- The Retail Manufacturing zone assumes 60% lot coverage and 2 stories.
- The Village Commercial (5) and (3) zones assume 80% lot coverage, minimum residential density of 35 and 25 dwelling units and 5 or 3 stories respectively.
- The Civic zone assumes 40% lot coverage and 2 stories.
- The General Commercial zone assumes 35% lot coverage and 2 stories.
- The Central Commercial zone assumes 80% lot coverage and 2 stories.
- The Residential/Professional zone assumes 50% lot coverage and a minimum residential density of 14.5 dwelling units per acre.
- The Residential Medium/High zone assumes a minimum residential density of 14.5 dwelling units per acre
- The Commercial zone assumes 40% coverage and 2 stories.

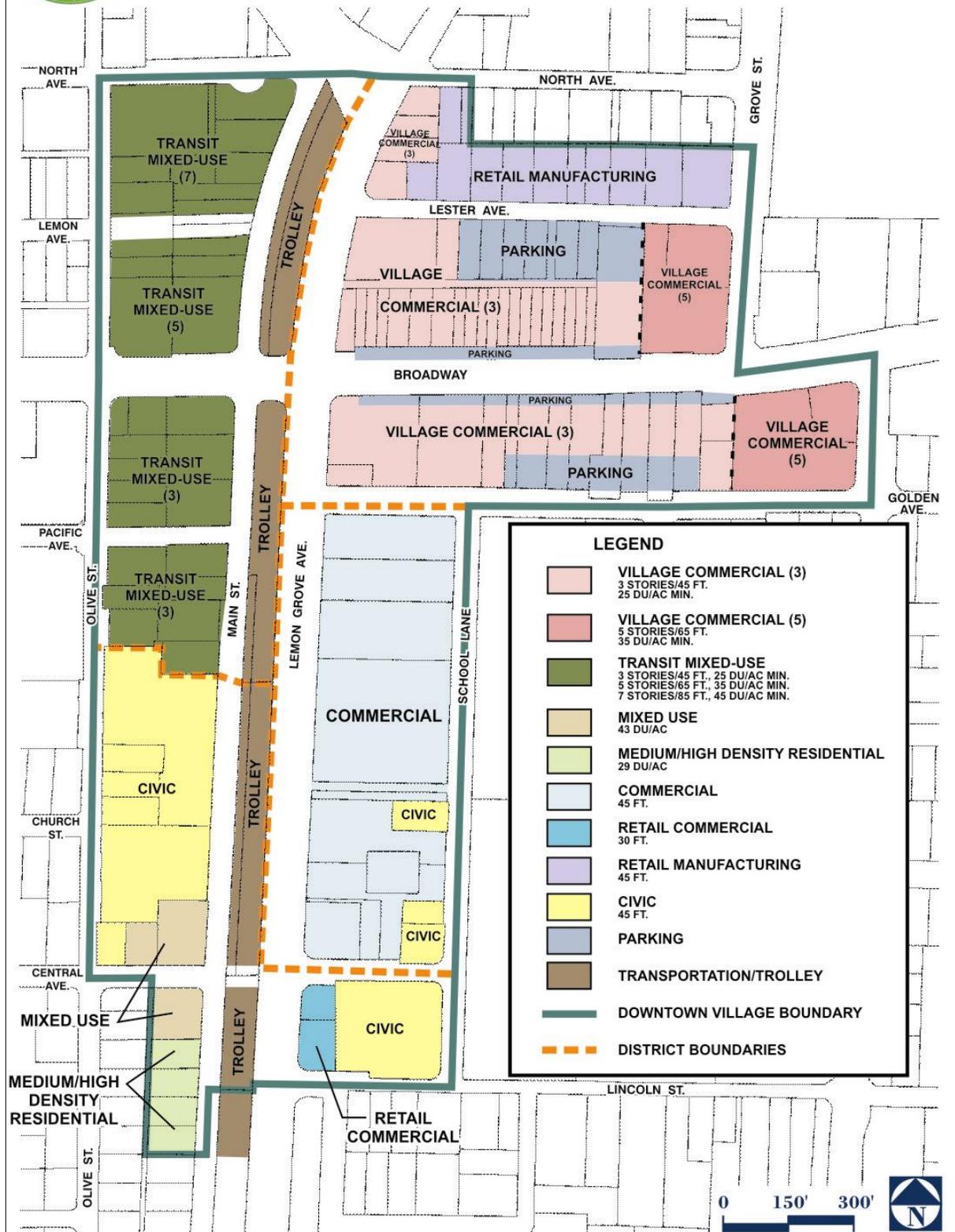
| Table 2. Building Area and Dwelling Units by District/Type of Use | | |
|--|------------------|----------------|
| District/Type of Use | Building Sq. Ft. | Dwelling Units |
| Main Street Promenade | | |
| Commercial ¹ | 135,560 | |
| Office ² | 210,482 | |
| Residential ³ | | 262 - 393 |
| Village Core | | |
| Retail Manufacturing | 129,112 | |
| Commercial ¹ | 275,560 | |
| Office ² | 373,134 | |
| Residential ³ | | 268 - 402 |
| Civic Center | | |
| Civic | 151,338 | |
| Commercial ¹ | 22,008 | |
| Office ² | 14,230 | |
| Residential ³ | | 19 - 29 |
| Marketplace | | |
| Commercial | 227,209 | |
| Civic | 17,773 | |
| Total Residential | | 546-819 |
| <p>Note: A mixture of Commercial, Office and Residential land uses are allowed in the three Transit Mixed Use and the two Village Commercial zones. See Table 1 for lot area by zone.</p> <ol style="list-style-type: none"> Commercial land use assumes a first story floor area ratio of 0.5 in the three Transit Mixed Use zones, 1.0 in the Village Commercial (5) zone, 0.75 in the Village Commercial (3) zone, and 0.50 in the Residential/Professional zone. Also assumes floor area ratio of 0.2 in the General Commercial zone and 1.6 in the Central Commercial zone. Office land use assumes a floor area ratio of 1.0 in the Transit Mixed Use (7) and (5) zones, and 0.50 in the Transit Mixed Use (3) zone; a floor area ratio of 2.0 in the Village Commercial (5) zone, 0.50 in the Village Commercial (3) zone, and 0.50 in the Residential/Professional zone. Residential land use assumes a minimum residential density in each zone. The maximum number of dwelling units assumes 150% of the minimum density. | | |

| Table 3. Building Area by Type of Use | |
|---------------------------------------|------------------|
| Use | Sq. Ft. |
| Commercial | 789,449 |
| Office | 579,846 |
| Civic | 169,111 |
| Total | 1,538,406 |





Land Use Plan



LAND USE PLAN



Figure 3

Uses within each Zone are either allowed by right or require review and approval by the City of Lemon Grove. The following describes Permitted Uses and Discretionary Use Permits. A list of Permitted Uses and uses allowed with a Discretionary Use Permit are provided separately for each of the four Districts. Where a use is not listed, the Director shall be responsible to make a determination of uses that are consistent with this Specific Plan. Unless the proposed use is determined to be similar to a listed use, it shall be prohibited. A Planned Development Permit is required for projects proposing demolition or construction of structures.

Permitted Uses are uses that are allowed within a zone without a Discretionary Use Permit. All other required applicable permits and approvals, a business license, building and other inspections, and a certificate of occupancy shall be obtained prior to commencement of the use.

Discretionary Use Permits are classified as Planned Development Permits, Conditional Use Permits, and Minor Use Permits that allow uses listed in this Specific Plan pursuant to the procedures provided for in Section 17.28 of the Municipal Code prior to issuance of a building permit or commencement of the use.

The **Planned Development Permit** procedure is intended to provide a method where by land may be designed and developed as a unit by taking advantage of modern site planning techniques, and to produce an environment of stable, desirable character which will be in harmony with the existing or potential development of the surrounding neighborhood; and standards of environmental quality, public health and safety, and efficient use of the City's resources.

Planned Development Permits are required for projects that propose demolition of an existing structure or construction of a new structure. Procedures for a Planned Development Permit are provided in Section 17.28 of the Municipal Code.

Sustainable Practices

The City strongly encourages the use of green building practices, sustainable development and energy, and green technology for new development and redevelopment. Green practices and sustainable development promote the wise use of materials, water and energy conservation and creating a safe and healthy environment while saving money in the long-term. The City also encourages a focus on lowering greenhouse gas (GHG) emissions, fostering renewable energy sources and water conservation efforts. Examples of these concepts would include (but are not limited to): solar/photovoltaic panels, wind turbines, recycled building products, low impact development standards, drip irrigation, drought tolerant plants, green roofs, access to mass transit, high density housing, recycled water systems, green space, etc.



B. Districts

1 – MAIN STREET PROMENADE

a. Purpose

The vision for the Main Street Promenade District is to create a grand promenade with wide pedestrian spaces, large trees, attractive landscaping, enhanced paving, and a transit center integrating both the trolley and bus service. Transit-oriented development (TOD), including a mix of land uses and increased intensity of existing development, as well as improved pedestrian linkages, will create a vibrant downtown. Redevelopment and enhancement of businesses within the District is intended to enhance the character of the long established businesses located on Main Street.

This District will be oriented around the trolley station located on Main Street north of Broadway. The Transit Station is a central element of the Main Street Promenade design concept. The promenade concept integrates the Transit Station, bus stops, future businesses and residences though convenient, safe, attractive and accessible pedestrian paths and plazas. The concept eliminates all vehicular traffic circulation on Main Street between Broadway and North Avenue and eliminates through traffic from Pacific Avenue onto Broadway via Main Street. This promenade concept expands the pedestrian realm in the Main Street Promenade District and provides open space park areas for future residents in the area, for commuters and for Lemon Grove residents, business owners and customers. Main Street between North Avenue and Broadway is vacated as a public street and this area is used for future development footprint and public open space, pedestrian pathways and plazas. Introduction of high-density residential uses with convenience retail businesses fronting the Main Street Promenade and Broadway are proposed. Lemon Avenue may be extended from Olive Street to Main Street as a private drive allowing vehicular access to the interior of the block and to a future Transit Station for a “kiss and ride” drop-off area. Parking

requirements in this District may be reduced due to the proximity of the Transit Station.

Circulation on Main Street south of Broadway will be accomplished by vacating portions of the street, thereby creating space for pedestrian only activity. With the right-of-way made available for pedestrian use, Main Street will be transformed into a pedestrian-friendly space with interconnections to bus stops, the Transit Station, the Civic Center District, and transit-oriented land uses along Main Street.



b. Land Use Descriptions

This District is intended to take advantage of the mobility and commuting convenience provided by the existing Trolley Station and planned Integrated Transit Station. The following transit-oriented land uses are shown on Figure 3.

i. Transit Mixed Use: This mixed use area allows a mixture of retail commercial uses oriented to commuters and local residents, office uses that would benefit from the proximity to public transit and high-density residential uses. When mixed uses occur in the same building, the ground floor fronting the street shall be devoted to retail or office uses, while using the area behind the retail or office uses and the upper floors for residences or other allowed uses. Underground or tuck-under parking shall be featured on site. Minimum residential densities are specified for the variety of building height limits in the three different zones.



ii. Transportation/Trolley: The Transit Station is located north of Broadway within the existing MTS San Diego Trolley right-of-way located between Main Street and Lemon Grove Avenue. The Metropolitan Transit System is considering raising the station platform to service low-floor trolley vehicles that accommodate passengers on wheelchairs more efficiently. The future designs for the Transit Station should function with the low-floor vehicles as well as the future grade separation of the trolley at Broadway and Lemon Grove Avenue. Control of the movement of the trolley across both Broadway and Lemon Grove Avenue with traffic signals is necessary until the future grade separation is completed.

c. Zones, Uses & Development Standards

This District provides for a mix of uses compatible with the Main Street Promenade, the Transit Plaza and Station and the pedestrian-friendly linkage to the Civic Center District.

i. Transit Mixed Use (7, 5 & 3) Zones

These three Zones are intended to provide for a mix of retail commercial, office and residential development that take advantage of the convenient access to the nearby bus and trolley transit services planned for the Integrated Transit Station. Retail commercial uses located at ground level will provide goods and services for both residents and commuters. Three different building height limits provide for a variety of minimum residential densities. (Figure 3.1)

Permitted Uses

The following uses are permitted, subject to the Development Standards in this Section and elsewhere in the Downtown Village Specific Plan.

1. Multi-family dwellings.
2. Retail sales activities, generally of a small-scale nature and oriented to the local residents and commuters (e.g., florists, grocery stores, convenience food stores,

baked goods stores, hobby shops, variety stores, candy stores, gift stores, pharmacy); but excluding automobile related retail activities, and building supply sales.

3. Retail service activities (e.g., laundromats, laundry and dry cleaning pick-up service, barber and beauty shops, shoe repair, photocopy and packaging services with postal contract services); excluding automobile repair and large appliance repair.
4. Restaurants without liquor sales or live entertainment; bars and cocktail lounges are not permitted.
5. Business, professional and technical offices. These may include accountants, advertising agencies, architects, attorneys, contractors, clerical agencies, contractors, doctors, engineers, financial consultants/institutions, insurance agencies, interior decorators, photographers, realtors and graphic artists.

Uses Requiring Discretionary Use Permits

These uses are allowed in the Main Street Promenade District with a Minor Use Permit or Conditional Use Permit. While these uses are not prohibited, they require review to ensure compatibility with the goals and objectives of this Specific Plan.

Minor Use Permit

The following uses are permitted subject to the approval of a Minor Use Permit according to the provisions of Section 17.28.052 of the Municipal Code:

1. Outdoor dining as an accessory use to a principal permitted restaurant, eating or drinking establishment located on the same lot or on the adjacent public right-of-way.
2. Restaurants and miscellaneous eating places when alcoholic beverages are to be sold.
3. Bed and Breakfast establishments.



4. Telecommunication and cellular telephone communications facilities.

Conditional Use Permit

The following uses are permitted subject to the approval of a Conditional Use Permit according to the provisions of Section 17.28.050 of the Municipal Code.

1. Public service, utility structures and facilities.
2. Animal Clinics.
3. Senior citizen housing.
4. Day Care and residential care facilities for six or fewer persons.
5. Clinics and other facilities providing professional on-site services to groups.
6. Cocktail lounges, bars and similar drinking establishments.

Planned Development Permit

The Planned Development Permit is a requirement for any project that proposes demolition of an existing structure or construction of a new structure.

The Planned Development Permit procedure is intended to provide a method whereby land may be designed and developed as a unit by taking advantage of modern site planning techniques; and to produce an environment of stable, desirable character which will be in harmony with the existing or potential development of the surrounding neighborhood; and to ensure development which meets standards of environmental quality, public health and safety, and efficient use of the City's resources.

Development Standards - Transit Mixed Use (7, 5 & 3)

This section establishes site development standards, land use regulations, and other general requirements that apply to all projects in

the three Transit Mixed Use Zones. Any standards not specifically covered by this Specific Plan are subject to the regulations of the Municipal Code.

1. Minimum Lot Area

Transit Mixed Use (7) – 20,000 square feet.
Transit Mixed Use (5) – 15,000 square feet.
Transit Mixed Use (3) – 10,000 square feet.

2. Minimum lot width and depth

Transit Mixed Use (7) – 100 feet wide and 160 feet deep.
Transit Mixed Use (5) – 80 feet wide and 140 feet deep.
Transit Mixed Use (3) – 60 feet wide and 120 feet deep.

3. Building Setbacks

The intent of this Development Standard is the creation of a strong street edge and the removal of rear buildings.

Main Street- Buildings, including designated outdoor use areas, shall be located within one foot of the ultimate right-of-way.

Broadway- Buildings, including designated outdoor use areas, shall be located within one foot of the ultimate right-of-way.

Pacific Avenue- Buildings, including designated outdoor use areas, shall be located within one foot of the ultimate right-of-way.

Lemon Avenue Extension- Buildings, including designated outdoor use areas, shall be located within one foot of the ultimate right-of-way or private easement. A “Kiss and Ride” drop-off area should be considered in the design of this road segment.

North Avenue- Buildings, including designated outdoor use areas, shall be located no farther than 5 feet from the ultimate right-of-way.

Olive Street- Buildings, including designated outdoor use areas, shall be located no farther



than 5 feet from the ultimate right-of-way.

4. Maximum Building Height

Transit Mixed Use (7) – 7 stories and/or 85 feet.

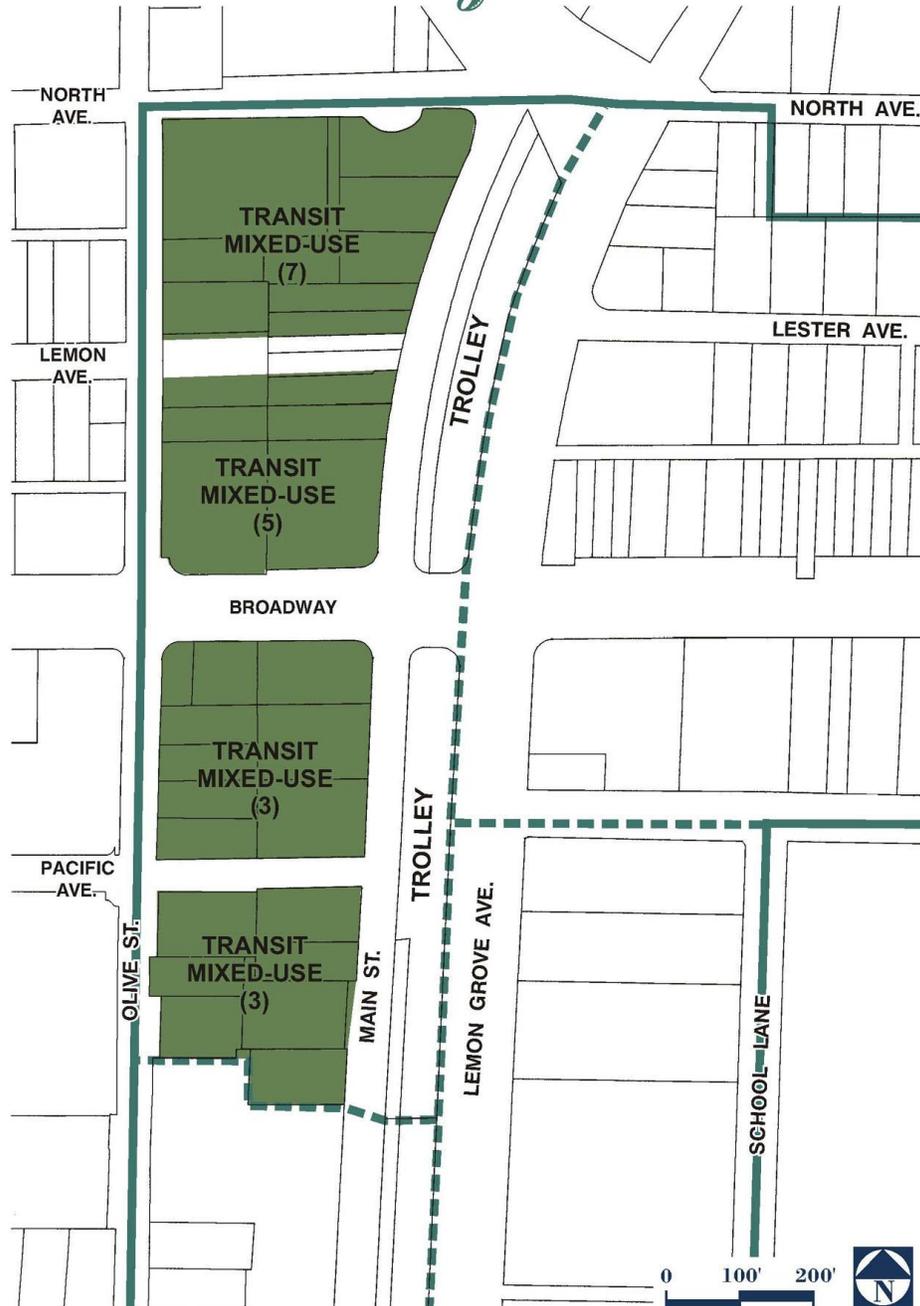
Transit Mixed Use (5) – 5 stories and/or 65 feet.

Transit Mixed Use (3) – 3 stories and/or 45 feet.





Main Street Promenade Zones



MAIN STREET PROMENADE ZONES
Figure 3.1



5. Off-Street Parking

Table A shows the parking calculations for the Main Street Promenade District. This District is calculated to need 1,517 parking spaces. This requirement is based on the factors discussed below.

The basic parking rate is based on the size of the building and/or the number of dwelling units and includes a reduction for transit. Additional reductions are made for the mixed-use nature of the area and for shared parking. This table also assumes an average of two bedroom dwelling units, and all residential parking shall be in designated spaces not available to the public.

Parking Rates - The minimum parking rates listed below are specific to the Main Street Promenade District. They reflect reductions from standard suburban rates due to the

accessible public transit available in the immediate area. Additional reductions for mixed uses and shared parking are possible.

- a. Residential (spaces per dwelling unit)
 - i. Studio – 1.0
 - ii. 1 bedroom – 1.25
 - iii. 2 bedroom – 1.75
 - iv. 3 bedroom – 2.0
- b. Office - 3.0 spaces / 1,000 square feet
- c. Retail - 4.3 spaces / 1,000 square feet
- d. Restaurant - 8.0 spaces / 1,000 square feet

Mixed Use Reduction - The mixed-use nature of the area and potential redevelopment warrant a mixed-use parking rate reduction. The combination of residential, office, and retail uses allows a 20 percent reduction in the above parking rates

| Use | Size | Parking Rate | Gross Parking Demand | Mixed-Use Reduction | Shared Parking Reduction @ Noon | Net Parking Demand |
|--|-----------|--------------|----------------------|---------------------|---------------------------------|---------------------|
| Transit MU 7 | | | | | | |
| -Retail | 34,709 SF | 4.3 /KSF | 149 | 0.80 | 1.00 | 119 |
| -Restaurant | 8,677 SF | 8.0 /KSF | 69 | 0.80 | 1.00 | 56 |
| -Office | 86,771 SF | 3.0 /KSF | 260 | 0.90 | 0.90 | 211 |
| -Residential ⁽¹⁾ | 112 du | 1.75 /du | 196 | 1.00 | 1.00 ⁽²⁾ | 196 |
| | | | 675 | | | 582 |
| Transit MU 5 | | | | | | |
| -Retail | 25,230 SF | 4.3 /KSF | 108 | 0.80 | 1.00 | 87 |
| -Restaurant | 6,308 SF | 8.0 /KSF | 50 | 0.80 | 1.00 | 40 |
| -Office | 63,075 SF | 3.0 /KSF | 189 | 0.90 | 0.90 | 153 |
| -Residential ⁽¹⁾ | 63 du | 1.75 /du | 110 | 1.00 | 1.00 ⁽²⁾ | 110 |
| | | | 458 | | | 391 |
| Transit MU 3 | | | | | | |
| -Retail | 48,509 SF | 4.3 /KSF | 209 | 0.80 | 1.00 | 167 |
| -Restaurant | 12,127 SF | 8.0 /KSF | 97 | 0.80 | 1.00 | 78 |
| -Office | 60,636 SF | 3.0 /KSF | 182 | 0.90 | 0.90 | 147 |
| -Residential ⁽¹⁾ | 87 du | 1.75 /du | 152 | 1.00 | 1.00 ⁽²⁾ | 152 |
| | | | 640 | | | 544 |
| Grand Total | | | 1,773 spaces | | | 1,517 spaces |
| Notes: | | | | | | |
| (1) Assume average of two bedroom units. | | | | | | |
| (2) Assume residential has designated parking not available for sharing. | | | | | | |
| SF = square feet (gross) | | | | | | |
| KSF = 1,000 square feet | | | | | | |
| du = dwelling unit | | | | | | |



Shared Parking- This is assumed to occur in the Main Street Promenade District. Shared parking is a time of day phenomena. Reductions for shared parking would be applied to the above parking requirements.

The combination of retail and office uses dictates that they control the parking situation over the residential uses. Therefore, the peak parking demand is expected to occur on a weekday between 11:00 AM and 2:00 PM.

In-Lieu Fees- This is an opportunity for redevelopment projects to purchase off-site parking when on-site parking cannot reasonably be provided. It is usually applicable in small or narrow lot situations, like the Village Core, where providing on-site parking would remove the building that is generating the parking need and/or the lot is too small for the physical maneuvering of cars. New projects shall be required to provide code required parking on site, but there are situations where it is more efficient for several new projects to provide a single common lot than smaller individual parking lots.

Up to 100 percent of the parking requirement could be satisfied through the payment of an in-lieu fee to be established by the City Council.

On-Street Parking- Credit for on-street parking can be taken from the calculated parking demand for a certain percentage of the parking supply present along a project's lineal frontage. This credit is available for all land use types. Metered parking cannot be included in the frontage available for a parking credit.

6. Service and Loading Space

Requirements – The minimum number of loading spaces below are specific to the Main Street Promenade District. The small-scale retail sales and services uses, and the type of office uses proposed require fewer and smaller service areas and loading spaces.

- a. Residential – No spaces; however, residential developments with 12 or more dwelling units shall provide a designated

service or loading area that can accommodate moving vans and appliance delivery vehicles.

- b. Office/Retail - 1 van space / 10,000-100,000 square feet. Additional space for each additional 200,000 square feet.
- c. Restaurant - 1 truck space / 80 seats

Van Loading Space – 14 feet wide by 20 feet long.

Truck Loading Space – 12 feet wide by 35 feet long.

- No portion of any front yard, or any side yard on the street side of a corner lot, shall be used for loading or service purposes.
- Fractional loading space requirements shall be rounded downward.
- No part of street shall be used for loading, except areas designated by the City for loading.

7. Street Elements

- Commercial sidewalk spaces and building setbacks along Main Street and the Main Street Promenade shall accommodate usable outdoor spaces to promote pedestrian activity. Sidewalk uses for outdoor seating or displays shall leave a minimum of 5 feet of unrestricted usable space for pedestrian circulation. Authorization from the City is required prior to using outdoor spaces for commercial activities.

8. Landscape

Street trees in public right-of-way. Landscaping in the public right-of-way is intended to create a distinct outdoor character within the Main Street Promenade and Main Street as well as accentuate the strong pedestrian linkages from the Transit Station to the Civic Center District. Generous sidewalk dimensions along Main Street and within the Main Street Promenade provide the opportunity for larger, wide canopy trees that provide shade and a sense of



enclosure. Street tree selection and any proposed substitutions to the recommended trees shall consider site and maintenance criteria such as planter area, sidewalk location, root damage potential, water and soil requirements, litter, hazards, longevity, and on-street parking location. A limited variety of trees are allowed to strengthen the individual character of the Main Street Promenade District.

The predominant trees located within the Main Street Promenade and the public right-of-way along both sides of Main Street and Broadway shall be large, broad canopy trees intended to create a more formal promenade. Trees shall be a minimum size of a 36-inch box and shall be planted on 40' centers in planting areas a minimum of 6 feet wide and 8 feet long. The entire planting area shall be low shrubs or ground cover. The Main Street Promenade landscape design shall create a park environment that provides visual continuity along the entire length of the promenade but also provides a variety of distinct gathering spaces linked by secure, convenient and accessible pathways. The recommended trees along Main Street are

- Podocarpus gracillior/African Fern Pine
- Magnolia grandiflora
- Plantanus ssp
- Tristania conferta/Brsibane Box
- Phoenix canariensis/Canary Island Date Palm

- Broadway's main tree characteristic consists of a large broad canopy tree:

- Podocarpus gracillior/African Fern Pine
- Tipuana tipu/Tipu Tree
- Pinus pinea/Italian Stone Pine
- Koelreuteria bipinnata/Chinese Lantern

- Trees located within the right-of-way along Olive Street and North Avenue shall have a more vertical characteristic to soften the multi-story buildings within the Main Street Promenade District from adjacent

development. Trees shall be a minimum size of a 24-inch box and shall be planted at the rate of one for every 25 feet of frontage. They shall be placed in planting areas a minimum of 4 feet x 6 feet in size. These trees shall include:

- Brachychiton acerifolius/Flame Tree
 - Hymenosporum flavum/Sweet Shade
 - Pinus canariensis/Canary Island Pine
 - Podocarpus macrophyllus/Yew Pine
- Other similar trees may be allowed with the approval of the Director.

- Trees located within the right-of-way along Pacific Avenue and the extension of Lemon Avenue shall be smaller in size with a broad canopy to provide shade and a sense of enclosure. Trees shall be a minimum size of a 24-inch box and shall be planted at the rate of one for every 25 feet of frontage. They shall be placed in planting areas a minimum of 4 feet x 6 feet. These trees shall include:

- Agonis flexuosa/Peppermint Tree
- Bauhinea v. candida/White Orchid Tree
- Bauhinea purpurea/Purple Orchid Tree
- Metrosidero excelsus/New Zealand Christmas Tree
- Prunus cerasifera/Purple-leaf Flowering Plum
- Pyrus kawakamii/Evergreen Pear

Other similar trees may be allowed with the approval of the Director.

Landscaping on private property. In any property development, an area creating comfortable and attractive outdoor areas shall include landscaping.

- All landscaping shall be shown on a detailed landscape plan approved by the Director.
- All planting areas shall be served by permanent irrigation systems.

9. Enclosures/Screening

- The screening for parking lots shall include at least four feet (4') of landscaped strip



between the vehicle area and the sidewalk with a 30” high screening element of either a dense hedge or a wall or fence integrated with shrub planting.

10. Signs

- Signs in the transom area above the door shall not exceed 65 percent of the width of the panel or 75 percent of the height of the panel.
- The height of letters shall not exceed 18 inches.
- Projecting signs shall be mounted on the structural piers of the building. They shall be no lower than 8’6” and no higher than the base of the building, or 14 feet, whichever is lower.
- Temporary advertising signs, sale signs, lease signs, etc. attached to the building and display windows shall be limited to 30 days of display time.

11. Nonconforming Use/Lot

Pursuant to Section 17.24.090 of the Municipal Code.

12. Temporary Use

Pursuant to Section 17.28.040 of the Municipal Code.

d. Design Guidelines – Main Street Promenade District

i. Site Planning and Street Design

Site planning and street design guidelines are developed to strengthen and preserve the pedestrian orientation of Main Street’s style. A “commercial strip” type of organization of buildings is not desired. The design intent applies to all commercial and residential street frontages within the Main Street Promenade District. A “main street” style is typified by

streetscape spaces that are pedestrian-oriented with strong “walkable” linkages between adjacent neighborhoods and public transportation.

The design intent for the Main Street Promenade is to create a pedestrian-only promenade and park as well as create a vibrant and exciting building environment along this edge. The design intent also builds upon the existing character of Downtown Lemon Grove found along Main Street between North Avenue and the Civic Center Complex. These guidelines support and encourage use of, build-to lines, massing, architectural styles and materials found in several representative buildings along Main Street. Styles include: Mission, Western Stick, Queen Anne (Trolley Station), Italianate and Moderne. Figure 3.2 shows examples of the Queen Anne style and the Stick style found in this District.



Figure 3.2 Architectural Styles

However, to encourage higher density transit-oriented housing, building facades and massing facing public-right-of-ways shall maintain a visual as well as physical pedestrian connection with the public-realm retain the “inviting” character of the District. Structures within a “core” of buildings may utilize modern building materials and systems.

The design intent is to create a “layer” of lower-scaled buildings at the street edge that blend in with the existing character of the District and



create a “stepped transition” from two to three story buildings at the perimeter of the streets to the taller buildings within the development core (Figure 3.3).

In addition, the creation of a Pedestrian Promenade within Main Street will help support Main Street’s appeal as a destination within San Diego County. The Promenade shall become a central feature within

Downtown Lemon Grove and shall help link both the Northern section of Main Street (north of Broadway) to the area south of Broadway. Figures 7 and 8-in Chapter IV 3.3, 3.3a, 3.4, and 3.5 illustrate the Promenade and new development concepts.

Additional street design criteria include:

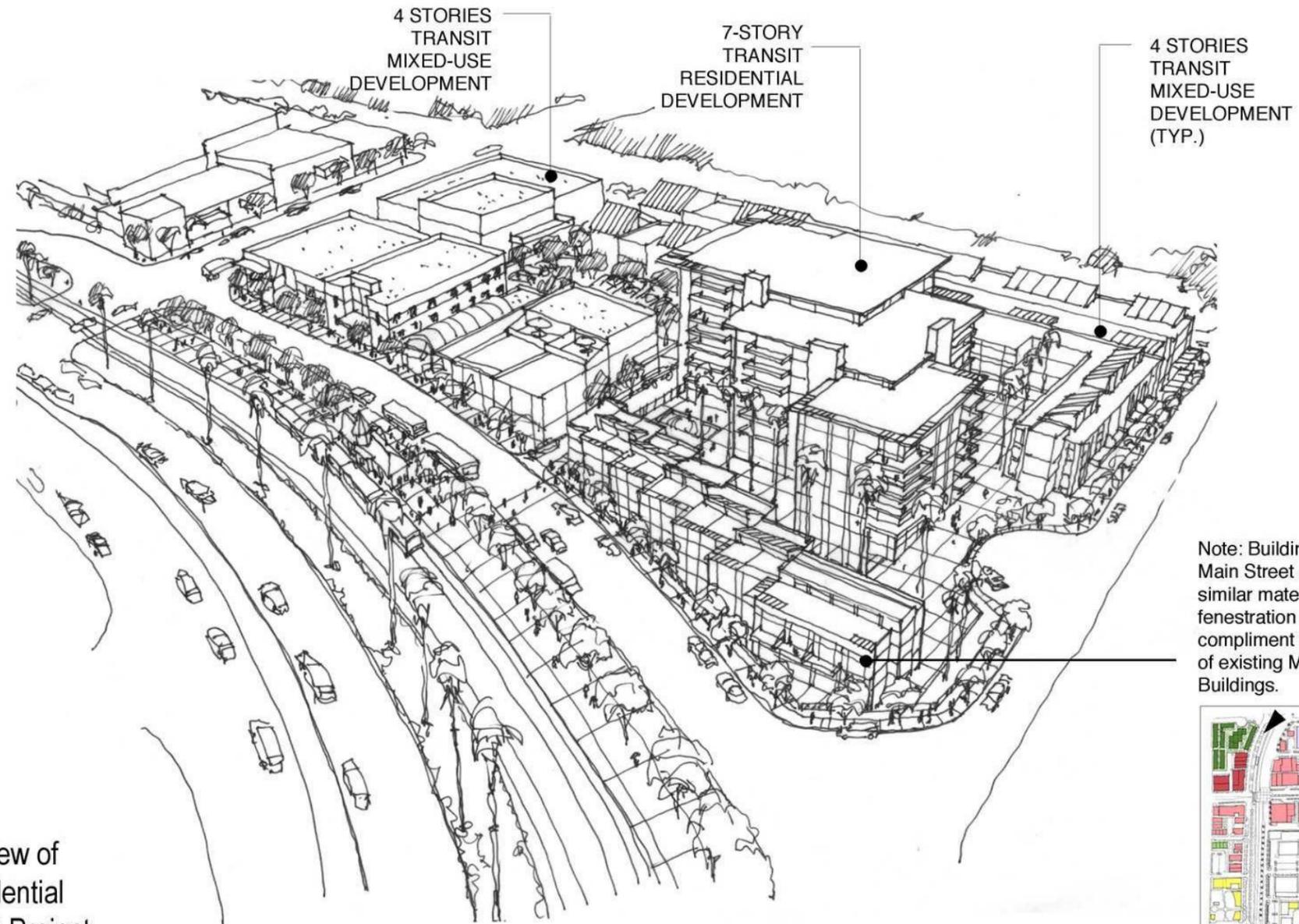
- The character of Main Street celebrates the street’s character as Downtown Lemon Grove’s original main thoroughfare. Redevelopment of the public right-of-way north of Broadway as a pedestrian only promenade shall strengthen this identity.
- Main Street buildings shall maintain a uniform setback and relationship to the pedestrian promenade as described in earlier sections of this chapter (Figure 7 & 8).
- The identity of Main Street south of Pacific Avenue shall be reinforced by a pedestrian-oriented streetscape including sidewalks, street lighting theme and a landscaped parkway with street trees. Along the western edge of Main Street, a minimum 20-foot wide sidewalk is recommended to allow room for outdoor spaces for restaurants, semi-public spaces, and planter areas.
- Alternative site design and open space options may be dictated by size and shape of the parcel and approved by the City of Lemon Grove.
- Scale and proportion of street and promenade frontage facades of new buildings shall compliment the scale and

proportion of existing buildings.

- Create public spaces and plazas at corners of new building by “carving” out building area in the site plan (Figure 3.6).
- At the street level, buildings shall maintain a uniform setback, as described in earlier sections of this chapter, to maintain the continuity of the “street wall.”
- Buildings shall be organized with primary entries oriented towards the principal street.
- Where feasible, provide pedestrian paseo linkages between buildings that link (see public spaces design guidelines in this section).
- Improve secondary access to buildings by providing secondary building entries from paseos, rear parking lots, and from side street building elevations.
- Service access and alleys shall be visually minimized from the Main Street Promenade, Main Street and Broadway and shall not be accessed directly from these streets.



Historic Main Street District



Bird's Eye View of
Transit Residential
Development Project



City of Lemon Grove Downtown Village Specific Plan

Specific Plan Urban Design Elements, January 14, 2004



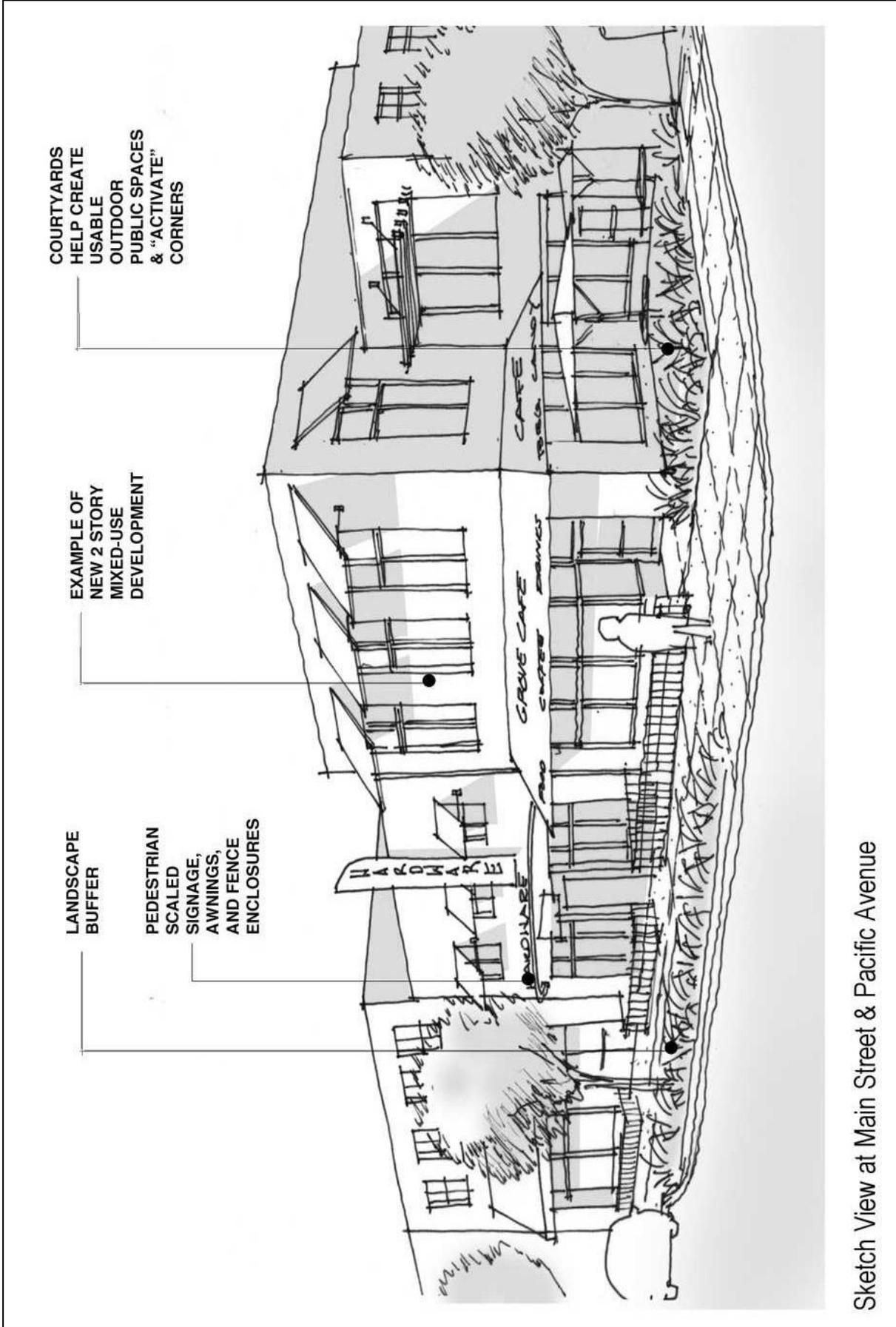
Roesling Nakamura Architects, Inc.

Figure 3.4



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COURTYARDS
HELP CREATE
USABLE
OUTDOOR
PUBLIC SPACES
& "ACTIVATE"
CORNERS

EXAMPLE OF
NEW 2 STORY
MIXED-USE
DEVELOPMENT

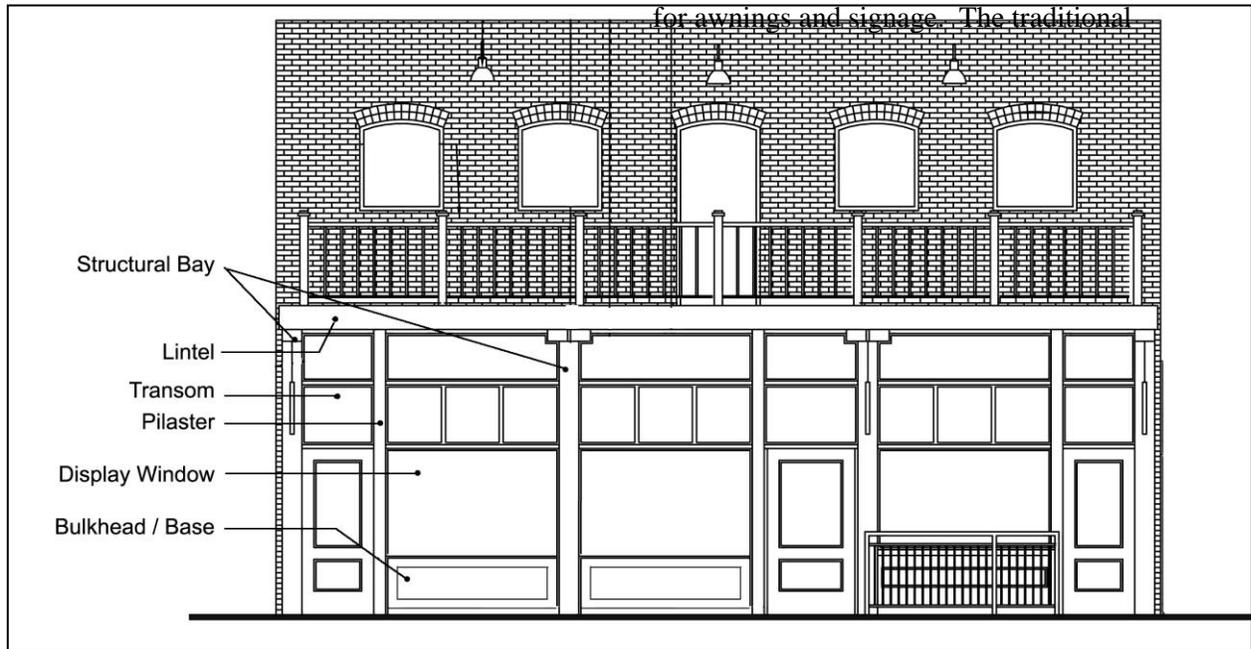
LANDSCAPE
BUFFER

PEDESTRIAN
SCALED
SIGNAGE,
AWNINGS,
AND FENCE
ENCLOSURES

Sketch View at Main Street & Pacific Avenue

Figure 3.6





ii. Storefront Design

The intent of storefront design guidelines is to provide criteria to promote quality design for the ground floor of new buildings and rehabilitation of existing buildings. The criteria presented below are intended to supplement the existing May 2001 Downtown Storefront Improvement Guidelines (See Appendix A). However, the additional guidelines presented in this section are to support and further develop the existing character of the Main Street Promenade.

Traditional building facades consisted of three elements: the storefront with a well-defined entrance and large display windows, the upper façade (typically masonry), with regular spaced windows, and the decorative cornice that caps the building (Figure 3.7). Existing buildings with these traditional facades may enhance the existing character by refurbishing the existing materials or by adding more contemporary features to accentuate the old and the new.

In traditional designs, storefronts had well-defined openings that the original storefront would fill. Typically, openings are bounded on the sides with structural piers such as masonry, wood, or stucco. Above these openings, well-defined lintel structures supported the upper portion of the building and created opportunities

Figure 3.7 Storefront Design

storefronts were constructed almost entirely of glass to display products and to allow natural lighting into the depths of the interior space.

Design guidelines for existing and proposed traditional storefront level facades for the Main Street Promenade include:

- The storefront is defined as that part of the building that comprises the front façade at ground level. Storefronts face the principal street. If a building is located on a corner lot, storefronts may be on both street faces.
- Large openings filled by windows and attractive window displays create a welcome ambience in a retail district and add interest to the pedestrian experience.
- At least 60 percent of the total area of the first floor façade facing Main Street shall be window display.
- Storefront openings shall extend to the structural piers on the sides and lintels above. Traditional methods of articulating these elements include the use of pilasters, entablatures, storefront cornices and sills.



- For the Main Street Promenade District the use of storefront transoms is encouraged. Transoms can assist with diffusing direct daylight into a space as well as provide operable openings to allow natural ventilation.
- Avoid flat facades by using recessed or projected entryways, bays, canopies, awnings and other architectural elements.
- Walls exposed to side streets must have doors, windows or fixed glazing areas of at least 30 percent of the total surface area of the wall facing the street.
- Architectural details on existing buildings help establish a historic building's distinct visual character; thus, they shall be preserved whenever feasible. If architectural details are damaged beyond repair, their replacement, matching the original detailing, is recommended.
- Traditional bulkheads, at the buildings base, shall be well articulated and are an important element of the storefront design. Materials that compliment the character of the building such as tile, wood, masonry are encouraged.
- No visible exterior security gates or bars are permitted unless architecturally integrated into the landscape and the structure. Interior security systems or enclosures shall not be visible from the exterior of the building during normal business hours.

iii. Landscaping/Hardscape

Landscape

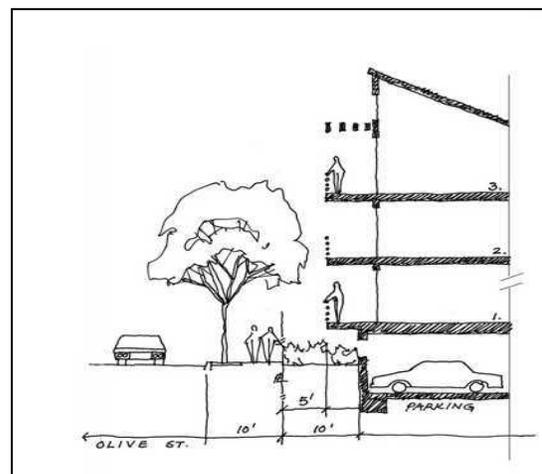
Landscape guidelines, incorporating both plants and paving, are intended to encourage the creation of comfortable and attractive outdoor spaces. This is accomplished by using street trees to either enclose, screen, shade or direct attention to the outdoor space. The use of a variety of paving materials and low planting areas create transitional zones between the

public areas of the street and the private spaces within buildings.

- The street tree plantings along Main Street are intended to create a welcoming outdoor linear space that provide a pedestrian-friendly linkage between North Avenue and the Civic Center District.
- Low shrub and groundcover plantings should be used to create transitions between the sidewalk and building entrances or outdoor private spaces.

Hardscape

- At the intersections of Main Street and Broadway and Main Street and Central Avenue, crosswalks shall utilize enhanced paving materials at the street surface to emphasize these intersections as gateways and nodes in the Main Street District.
- Public and semi-private spaces shall utilize enhanced paving systems such as brick, tile, and/or stamped concrete paving.
- For street-level residential development, create semi-private open spaces (e.g. front and rear yards, porches, stoops, and patios) that create a transition from public sidewalks to courts to private interior spaces (Figure 3.8).



**Figure 3.8
Residential Frontage Along Olive St.**

iv. Signage/Identity

The Signage criteria are intended to unify the graphic character of signs advertising a business within the Main Street Promenade District. The design intent is to provide signage that supports the character found within this district. For the Main Street Promenade District, the traditional approach for signage design is encouraged. This is typically defined as signage that is pedestrian-scaled, finely crafted with ample detailing, and oriented to enhance the pedestrian walking experience.

Within the Main Street Promenade District applicable signage standards apply to storefront façade signage that communicates the identity of a business from both the pedestrian level and from the street. Commercial signs are regulated by the City of Lemon Grove’s Sign Ordinance and must meet State and Federal Accessibility requirements.

Exterior Signs

Guidelines support a unique signage design theme along Main Street. Exterior signage guidelines for the Main Street Promenade District shall include the following:

General Criteria

- In pedestrian areas, such as the Promenade sidewalk along the west edge of Main Street, signage dimensions shall be ground level, pedestrian-scaled and shall demonstrate a high-level of craftsmanship (Figure 3.9).



Figure 3.9 Pedestrian Scale Signage

- Signage may be externally or internally illuminated.
- Businesses shall only include the formal name of the business, the nature of the business and the address. There shall be no advertising of brand names.
- To encourage diversity in design there are no specific fonts or letter style that are recommended. However, a letter style shall be chosen that is easy to read and that supports the Promenade signage character described above.
- Flashing, moving or rooftop signs are generally not permitted, unless reviewed and approved through the Planned Development Permit process.

Buildings Signs

- Storefront transoms or display windows shall not be covered by opaque signs or panels.
- Automobile-oriented external wall signs shall be proportionate to the wall area and not exceed fifty (50) square feet.
- Sign types shall fit within the architectural details of the building on which they are placed.

Pedestrian Oriented Signage

- Pedestrian-level display window signs applied directly to the glass shall consist only of lettering without an opaque background. On the display window, such a sign shall not cover more than 20 percent of the area of the glass panel(s).
- Hanging signs shall project no more than three feet from the face of the building. Brackets and other methods of attachment shall be considered part of the design proposal and shall be designed to be compatible with the building.

- Window signs shall not obscure the display area. The color of letters shall contrast with the display background. Light colored letters or gold leafed letters with dark borders are effective.
- Window signs shall not exceed 40 percent of the window surface area. For shops along Broadway, smaller pedestrian-oriented scaled window signs are encouraged.
- Display cases or interior partitions shall not obstruct windows.
- Provide a directional map structure for downtown at key public transportation locations (Figure 3.10).

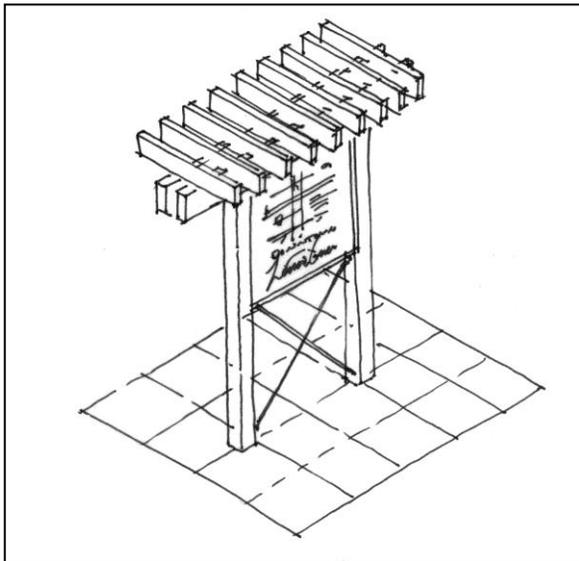


Figure 3.10 Directional Map Structure

Projecting Signs

- Larger automobile projecting building signs shall not exceed the City ordinance of one square foot of signage to two feet of building area with a maximum of fifty (50) square feet.
- Projecting signs shall compliment the architecture of a building in respect to colors, materials, and structure. Only one projecting sign shall be allowed per building.

- Projecting signs shall be made of traditional materials such as wood, wrought iron, steel or glass.
- All projecting signs shall be externally lit. External light sources shall be shielded from viewers on the street to prevent glare.
- Finely crafted pedestrian-scaled hanging signs are encouraged yet shall project no more than three feet from the face of the building and shall allow a minimal clear area for 7'-0" from the bottom of sign to the walking surface. Brackets and other methods of attachment shall be considered part of the design proposal and shall be designed to be compatible with the building (Figure 3.8).

Awnings

- Awnings or canopies shall reflect the door and window openings or structural bays of the building façade as described in the storefront guidelines above.
- To retain a sense of continuity along Main Street, awnings shall be of the traditional sloped configuration. Curved, vaulted or semi-spherical awnings are discouraged.
- To encourage a pedestrian scale along Main Street, street-level awnings shall provide coverage for the storefront area only including transom spaces. Large multi-story awnings are discouraged.
- Signs on awnings shall have contrasting letters painted or sewn onto the valance or skirt of the awning. Letters shall not exceed six to eight inches in height.
- To compliment the District's character, fabric awnings such as canvas or Neoprene impregnated fabric are preferred. Vinyl, metal, aluminum, wood, or preformed panels are not permitted. The underside of awnings shall be open to expose the structure (Figure 3.11).





Figure 3.11 Awnings

v. Public Spaces

The Main Street Promenade District has public spaces that include a promenade connecting the Integrated Transit Station and Plaza with bus stops and the Civic Center District. The linear park, small plazas, paseos and outdoor uses in the public right-of-way, especially along Main Street and within the promenade, will become significant elements of the urban landscape that preserve views from public streets to important area landmarks and provide convenient, safe and accessible pedestrian linkages to the Integrated Transit Station, parking areas and building entrances.

For residential buildings, front stoops, porches and low walls or planting areas create transitional zones between the public areas of the street or promenade and the private areas adjacent to the residential structures. For commercial areas, all elements visible from the public street, including landscape planting areas, building facades, signage are a part of the public space (Figure 3.12).



Figure 3.12 Public Spaces

vi. Streetscape Elements

Streetscape elements shall be the unifying feature of all four Downtown Districts.

Streetscape elements shall compliment the “Main street” theme and items shall include; tree grates, benches, directional signage, street name signage, freestanding information kiosks, trashcans, lighting fixtures, bus stop shelters and benches.

- Coordinate the design and placement of high-quality streetscape elements throughout all four districts.
- Adopt a public art program to strengthen the cultural identity of Lemon Grove. Public art sites shall be located within the promenade, pedestrian gathering locations, landmark locations, public courtyards and at District entries (Figure 3.13).



Figure 3.13 Public Art

- Install wayfinding and informational signage at key circulation locations and downtown entries that provide information to both vehicle and pedestrians regarding parking and other destinations to increase public understanding of the District (Figure 3.14).



Figure 3.14 Wayfinding/Informational Signage

- All service areas and trash containers must be screened from view through the use of evergreen plant materials, masonry screens or similar solid structures compatible with the building design.
- Appropriate landscape buffers shall be designed and maintained in parking areas to enhance the external view of parking area and to screen the parking from the adjacent residences. Landscape buffers can include landscaping, site walls or arcades.
- Design residential courtyards to be visually accessible from streets to enhance safety and encourage activity along the street.

Pedestrian “Paseos”

Where feasible at existing and planned open spaces between buildings that link to parking areas or adjacent neighborhoods, create pedestrian passageways also known as “Paseos.” Paseos allow property and business owners to increase their façade exposures to the public while providing safe pedestrian-scaled passageways between parking areas to the building frontages.

Design criteria include the following:

- Paseos shall be no less than 12’-0” in width.
- Side walls exposed to paseos must have windows or glazing areas of at least 30

percent of the total surface area of the wall facing the paseo.

- If feasible, include landscape planters, outdoor public seating, site walls, lighting, and benches within the paseo corridor.
- Shade structures such as fabric structures or lattice systems promote a pedestrian scaled environment while providing shade and cover during inclement weather.
- Projecting and hanging signs located along paseos shall project no more than three feet from the face of the building. Brackets and other methods of attachment shall be considered part of the design proposal and shall be designed to be sensitively compatible with the building.

Lighting

- Pedestrian lighting oriented for the public right-of-way and commercial properties shall compliment the existing lamp fixtures found within the Main Street Promenade District (Figure 3.15).

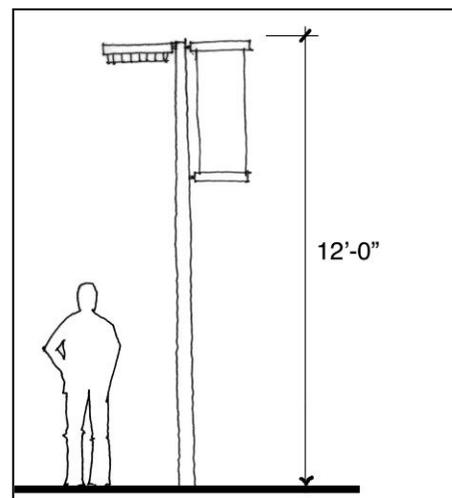


Figure 3.15 Pedestrian Lighting

- To compliment the character and facades of buildings facing Main Street, architectural lighting of front facades is encouraged.



Banners

- Banners on light poles are an effective way of adding visual interest and drawing attention to special civic events within Downtown Lemon Grove. Banners should reflect seasonal changes or announce special civic events.

vii. Parking

Parking and circulation guidelines are provided to enhance the identity of the Main Street Promenade District. Signage that will direct both vehicles and pedestrians to parking and other destinations within the downtown area shall be provided as well as adequate parking for future development. Design criteria include:

- Where feasible, provide a single, shared, rear-lot parking area located behind commercial buildings accessible from an adjacent street (Figure 3.16).
- Access to rear parking lots shall be from Olive Street, Pacific Avenue, North Avenue and Central Avenue only. No access to rear-lot parking lots shall be permitted from Main Street except at the boundary between this District and the Civic Center District.
- Provide clear, accessible and safe pedestrian landscaped pathways from the parking areas to principle commercial streets. Pathways shall have a minimum clear width of 60” and shall meet State and National accessibility requirements. Surface materials shall include concrete or other durable materials, such as cast-concrete pavers and/or brick.
- All parking lots shall incorporate landscaping and have a concrete curbed perimeter. Islands shall provide shade trees in the interior of the parking lot and there shall be a minimum of one shade tree per island; two trees per island are required where opposite bays align. Trees within islands shall be aligned between parking spaces.



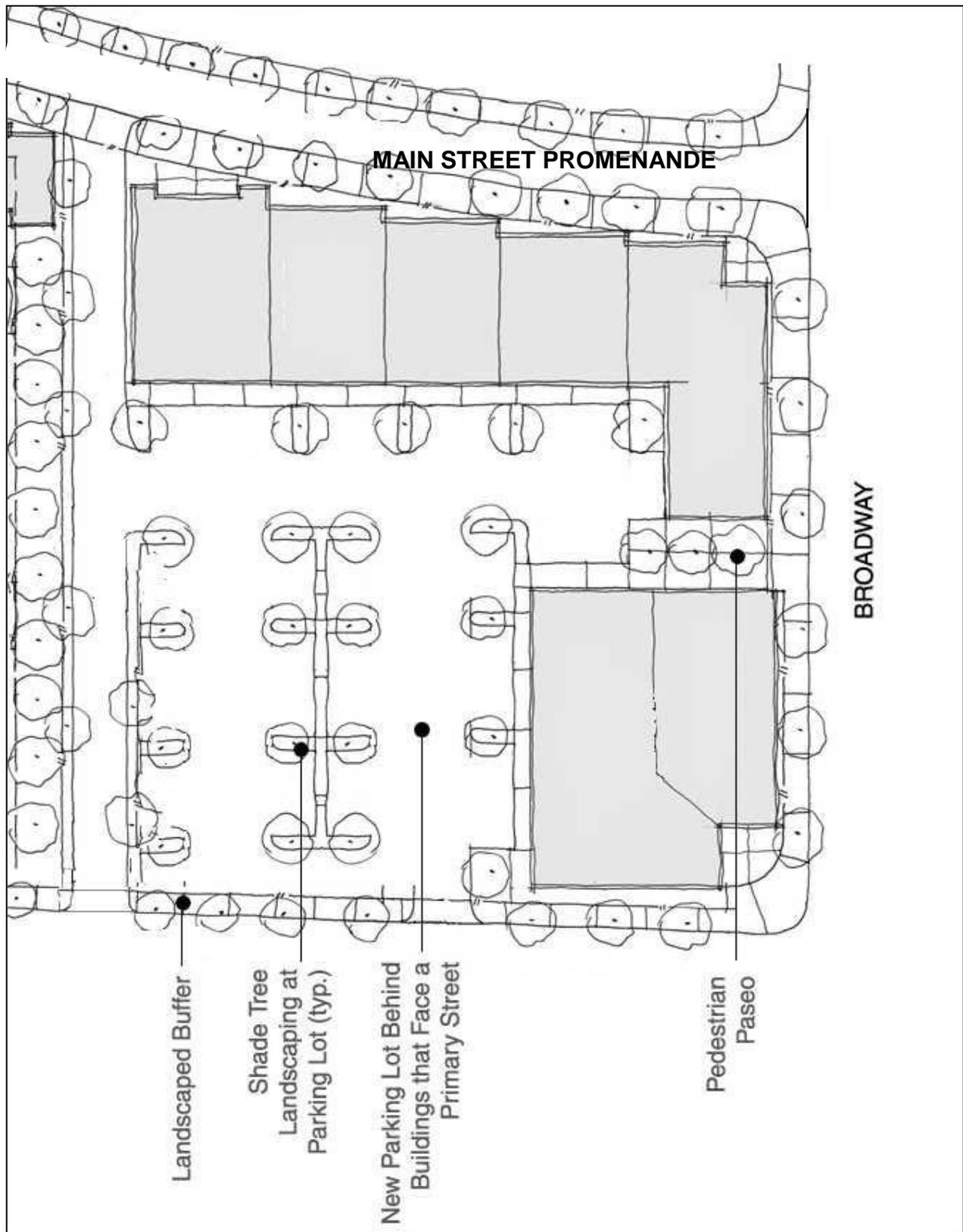


Figure 3.16 - Tuck Behind Parking



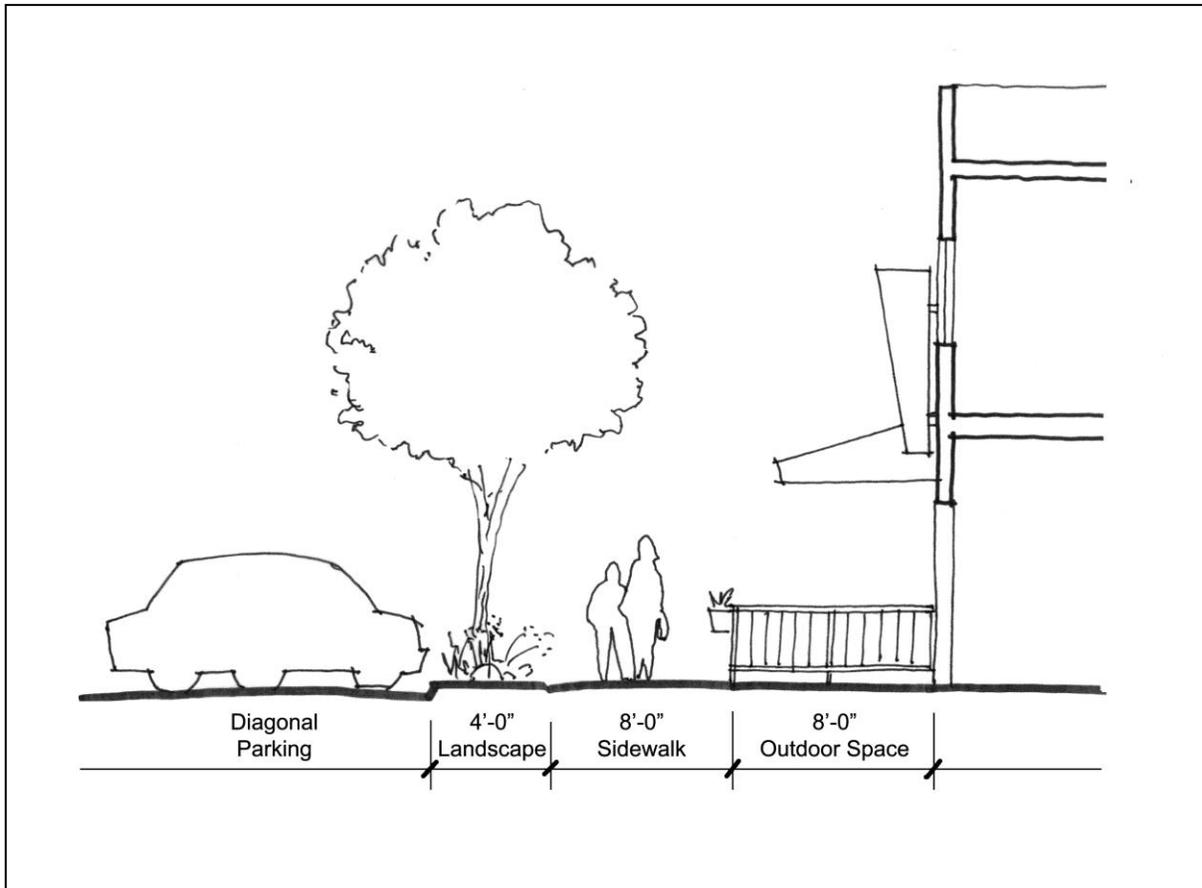


Figure 3.17 Parking Buffer

- Provide landscape buffers or screening elements that separate public right-of-ways from parking lots and help soften the visual impact of parking lots edges (Figure 3.17).
- Buffers or screens shall be composed of a minimum four feet (4') of landscaped strip between the vehicle area and the public sidewalk with a 30" high screening element of either a dense hedge, a wall or fence integrated with shrub planting. Landscape buffers may also include site walls or arcades (Figure 3.17).
- All parking areas shall provide adequate lighting at nighttime utilizing fixtures that support the scale, character and identity of the Main Street Promenade District (see lighting standards).
- Directional signage and/or information kiosks within parking lots are encouraged and shall be visually attractive, complimenting the scale, character and identity of the Main Street Promenade District (see signage guidelines for specific information).
- Parking spaces adjacent to landscape islands shall be two (2) feet wider than standard parking spaces.

VILLAGE CORE

a. Purpose

The character of the Village Core District is based on the existing lively mix of small community-serving retail uses. The intent of the Development Standards and Design Guidelines for this District is to preserve Lemon Grove's small town feel, while enhancing and unifying the retail commercial uses along Broadway and portions of Lemon Grove Avenue.

Pedestrian amenities are required to focus shoppers' attention towards shops and stores. Specialty retail and service uses shall be pedestrian friendly and are intended to serve the local community. Outdoor eateries, entertainment and retail shall be encouraged. Redevelopment and/or enhancement of storefronts shall follow the requirements of the May 2001 Downtown Storefront Improvement Guidelines. The addition of residential land uses that would be compatible with the existing mix of small community-serving retail uses is encouraged. Additional convenient parking for retail customers is needed.

The Retail Manufacturing land use will act as a transition between the Village Commercial land use and the existing industrial/manufacturing uses located to the north of Lester Avenue. Retail sales of goods and services produced or assembled on-site is encouraged in the Retail Manufacturing Zone.

b. Land Use Descriptions

The plan for this District preserves the lively mix of existing retail commercial uses that are able to use the existing convenient diagonal parking along Broadway and encourages the addition of residential uses above and behind the retail commercial uses. The Retail Manufacturing land use north of Lester Street will provide a transition compatible with the Village Commer-

cial land use and the mixed industrial uses fronting on North Avenue. The efficient utilization of existing and new public parking to be provided by the City of Lemon Grove is a vital component to the redevelopment of this District. See Figure 3.

i. Village Commercial: This is a mixed-use area allowing a mixture of retail and commercial oriented businesses to local residents, office, and residential uses within the same building, lot or area, with the intent of creating a lively pedestrian-oriented village near the planned Integrated Transit Station. Two different development standards provide for a variety of minimum residential densities.

ii. Retail Manufacturing: This land use includes light to moderate intensity manufacturing, assembly or commercial services, and encourages the on-site retail sales of goods produced or assembled. The retail sale of products similar, complimentary or accessory to the products manufactured, assembled or services on the site is allowed. Examples may include: furniture manufacturing with a retail showroom, reprographic services, and ceramics production with accompanying retail sale of ceramics.

iii. Parking: This land use includes both existing public parking and new parking intended to be acquired by the City of Lemon Grove.

c. Zones, Uses & Development Standards

This District provides for a mix of uses intended to preserve Lemon Grove's small town feel and to serve the local community.

i. Village Commercial (5, 3) Zones

These two Zones are intended to provide for a mix of retail commercial, office and residential development that serve the community. Two different development standards provide for a variety of minimum residential densities (Figure 4).



Permitted Uses

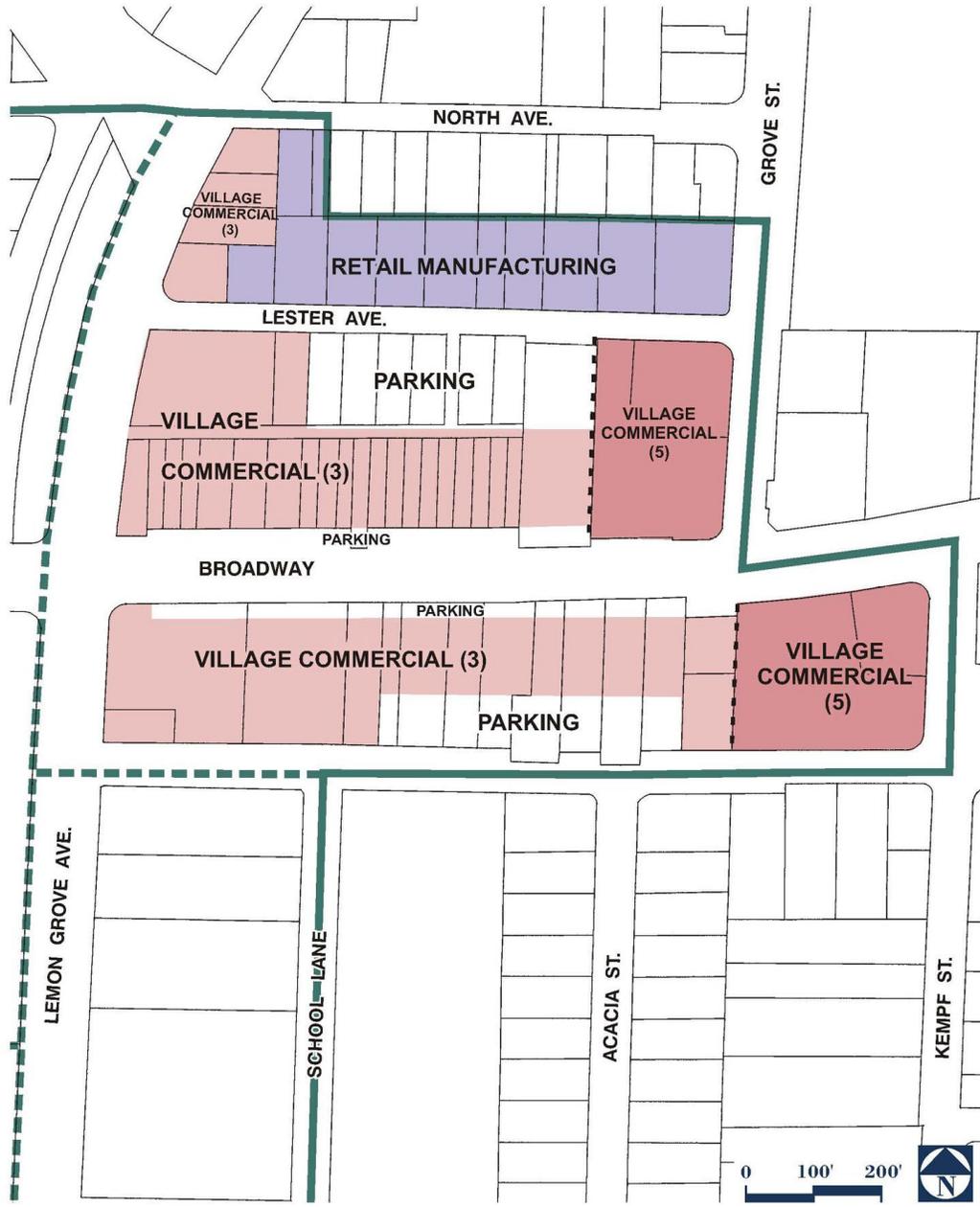
In the two Village Commercial Zones, the following uses are permitted, subject to the Development Standards in this Section and elsewhere in the Downtown Village Specific Plan.

1. Pedestrian-oriented retail sales activities which provide a mix of compatible uses that are supportive of an active mixed use downtown and offering specialty and comparison goods of a high quality, but space intensive nature suitable to a pedestrian-oriented downtown shopping district; including, but not limited to; apparel, jewelry, books, boutique and specialty shops, hardware stores without outdoor sales or display areas and health and beauty care items; but excluding second-hand merchandise with more than five percent (5%) of the total sales floor area.
2. Pedestrian-oriented retail services activities which provide a mix of compatible uses that are supportive of an active mixed use downtown and offering specialized services of a high quality and space intensive nature; including, but not limited to; hair styling, apparel repair, pet shops, photo processing, instant printing and private postal services, not including job printing uses involving printing presses or large commercial equipment; and excluding automobile services, job printing or equipment rental.
3. Business, professional and technical offices. (e.g., doctor, lawyer, architect, engineer, realtor, travel agent, accountant, insurance or security brokers, advertising agencies).
4. Financial Institutions.
5. Restaurants and miscellaneous eating places; but excluding drive-in facilities. When such businesses also provide the incidental sale of alcoholic beverages, a Minor Use Permit is required.
6. Cultural activities (e.g., library, museum, gallery).
7. Residential services (e.g. coin-operated laundromats, dry cleaning and laundry pick-up stations, grocery stores, drug stores) with a maximum floor area of 6,000 square feet; retail sales and service commercial uses supportive of residential land uses in the immediate area.
8. Any other retail business or service establishment which the Planning Commission or City Council finds to be consistent with the purposes of this Section and intent of the General plan and the Downtown Village Specific Plan.
9. Residential dwelling units above commercial establishments.
10. Outdoor dining as an accessory use to a principal permitted restaurant, eating or drinking establishment located entirely on the lot.





Village Core Zones



VILLAGE CORE ZONES
Figure 4



Uses Requiring Discretionary Use Permits

Minor Use Permit

The following uses are permitted subject to the approval of a Minor Use Permit according to the provisions of Section 17.28.052 of the Municipal Code:

1. Outdoor dining as an accessory use to a principal permitted restaurant, eating or drinking establishment located on the adjacent public right-of-way within the Downtown Village (STA 1) area.
2. Outdoor newsstands attended by salesperson(s); outdoor sale of flowers and similar outdoor activities, which are to be conducted on a permanent or regularly recurring basis.
3. Clinics and other facilities providing professional on-site services to groups.
4. Churches and similar places of religious assembly in commercial buildings or centers provided the total floor area of the church(es) or place(s) of religious assembly, whether separately or in combination do(es) not exceed 20 percent of the total floor area of the commercial building or center.
5. Other types of assembly spaces such as schools, vocational schools, lecture halls, service organizations.
6. Telecommunication and cellular telephone communications facilities.

Conditional Use Permit

The following uses are permitted subject to the approval of a Conditional Use Permit according to the provisions of Section 17.28.050 of the Municipal Code:

1. Public service, utility structures and facilities.
2. Hotels and associated uses.

3. Spectator entertainment assembly spaces and uses such as theaters and other forms of spectator entertainment (e.g., live music, plays, cinemas, night clubs, comedy clubs), excluding any use defined as an Adult Entertainment establishment pursuant to Section 18.28.020 (Adult Entertainment) of the Development Code.
4. Retail sales establishments offering auto parts, furniture, large appliances and liquor.
5. Senior citizen housing.
6. Cocktail lounges and similar drinking establishments.
7. Restaurants and miscellaneous eating places when alcoholic beverages are to be sold and which have separate bar or cocktail lounge as required by Section 18.27.060B(8).

Development Standards - Village Commercial (5 & 3)

This section establishes site development standards, land use regulations, and other general requirements that apply to all projects in the two Village Commercial Zones. Any standards not specifically covered by this Specific Plan are subject to the regulations of the Municipal Code.

1. Minimum Lot Area

Village Commercial (5) – 10,000 square feet.
Village Commercial (3) – 6,000 square feet.

2. Minimum lot width and depth

Village Commercial (5) – 75 feet wide and 140 feet deep.
Village Commercial (3) – 50 feet wide and 120 feet deep.

3. Building Setbacks

The intent of this Development Standard is the creation of a strong street edge and the removal of rear buildings.



- **Broadway – north and south sides**
Village Commercial (5) - Buildings shall be located within one foot of the ultimate right-of-way.

Village Commercial (3) - Buildings shall be located within one foot of the ultimate right-of-way.

- **Golden Avenue – north side**
Village Commercial (5) - Buildings shall be located within 10 feet of the ultimate right-of-way.

Village Commercial (3) - Buildings shall be located within 10 feet of the ultimate right-of-way, west of School Lane and no closer than 84 feet from the ultimate right-of-way east of School Lane.

- **Lester Avenue – north and south sides**
Village Commercial (5) - Buildings shall be located within one foot of the ultimate right-of-way.

Village Commercial (3) - Buildings shall be located within 20 feet of the ultimate right-of-way.

- **Lemon Grove Avenue – east side**
Village Commercial (3) - Buildings shall be located within one foot of the ultimate right-of-way.

- **Grove Street – west side**
Village Commercial (5) - Buildings shall be located within one foot of the ultimate right-of-way.

- **Kempf Street – west side**
Village Commercial (5) - Buildings shall be located within one foot of the ultimate right-of-way.

- **North Avenue – south side**
Village Commercial (3) - Buildings shall be located within one foot of the ultimate right-of-way.

4. Maximum Building Height

- Village Commercial (5)- 5 stories or 65 feet.
- Village Commercial (3)- 3 stories or 45 feet.

5. Mixed Use within same building on same lot

When mixed uses occur in the same building, the ground floor fronting Broadway shall be devoted to retail commercial or office uses.

6. Minimum Usable Open Space

- Residential- balconies of fifty (50) square-feet (min. dimension of 10 feet in width and 5 feet in depth).
- Minimum required common open space per unit shall be 350 square feet per dwelling unit.

7. Off-Street Parking

Table B shows the parking calculations for the Village Core District. This District is calculated to need 2,754 parking spaces. This requirement is based on the factors discussed below. The basic parking rate is based on the size of the building and/or the number of dwelling units and includes a reduction for transit. Additional reductions are for the mixed-use nature of the area and for shared parking. This table also assumes an average of two bedroom dwelling units, and all residential parking is in designated spaces not available to the public.

Parking Rates - The minimum parking rates listed below are specific to the Main Street Promenade District. They reflect reductions from standard suburban rates due to the accessible public transit available in the immediate area. Additional reductions for mixed uses and shared parking are possible.



**Table B
Village Core**

| Use | Size | Parking Rate | Gross Parking Demand | Mixed-Use Reduction | Shared Parking Reduction @ Noon | Net Parking Demand |
|-----------------------------|------------|--------------|----------------------|---------------------|---------------------------------|---------------------|
| Retail Manufacturing | 129,112 SF | 2.0 /KSF | 258 | 1.00 | 1.00 | 258 |
| Village Commercial 5 | | | | | | |
| –Retail | 78,059 SF | 4.3 /KSF | 336 | 0.80 | 1.00 | 269 |
| –Restaurant | 19,515 SF | 8.0 /KSF | 156 | 0.80 | 1.00 | 125 |
| –Office | 195,148 SF | 3.0 /KSF | 585 | 0.90 | 0.90 | 474 |
| –Residential ⁽¹⁾ | 98 du | 1.75 /du | 172 | 1.00 | 1.00 ⁽²⁾ | 172 |
| | | | 1249 | | | 1039 |
| Village Commercial 3 | | | | | | |
| –Retail | 142,389 SF | 4.3 /KSF | 612 | 0.80 | 1.00 | 490 |
| –Restaurant | 35,597 SF | 8.0 /KSF | 285 | 0.80 | 1.00 | 228 |
| –Office | 177,986 SF | 3.0 /KSF | 534 | 0.90 | 0.90 | 433 |
| –Residential ⁽¹⁾ | 170 du | 1.75 /du | 298 | 1.00 | 1.00 ⁽²⁾ | 298 |
| | | | 1729 | | | 1448 |
| Grand Total | | | | 3,235 spaces | | 2,745 spaces |

Notes:
(1) Assume average of two bedroom units.
(2) Assume residential has designated parking not available for sharing.
SF = square feet (gross)
KSF = 1,000 square feet
du = dwelling unit

- a. Residential (spaces per dwelling unit)
 - i. Studio – 1.0
 - ii. 1 bedroom – 1.25
 - iii. 2 bedroom – 1.75
 - iv. 3 bedroom – 2.0
- b. Office - 3.0 spaces / 1,000 square feet
- c. Retail - 4.3 spaces / 1,000 square feet
- d. Restaurant - 8.0 spaces / 1,000 square feet

Mixed Use Reduction- The mixed-use nature of the area and potential redevelopment warrant a mixed-use parking rate reduction. The combination of residential, office, and retail uses allows a 20 percent reduction in the above parking rates for the retail and restaurant uses. A 10 percent reduction for office uses is allowed.

Shared Parking- This is assumed to occur in the Village Core District. Shared parking is a time of day phenomena, while mixed use is a reduction due to multiple activities that can be accomplished by parking once. Reductions for shared parking would be applied to the above parking requirements.

The combination of retail and office uses dictates that they control the parking situation over the residential uses. Therefore, the peak parking demand is expected to occur on a weekday between 11:00 AM and 2:00 PM. A 10 percent reduction for office uses is given.

In-lieu fees- This is an opportunity for redevelopment projects to purchase off-site parking when on-site parking cannot reasonably be provided. It is usually applicable in small or narrow lot situations, where providing on-site parking would remove the building that is generating the parking need and/or the lot is too small for the physical maneuvering of cars. New projects shall be required to provide Code required parking on site, but there are situations where it is more efficient for several new projects to provide a single common lot than smaller individual parking lots.

Up to 100 percent of the parking requirement could be satisfied through the payment of an in-lieu fee to be established by the City Council.



On-Street Parking- Credit for on-street parking can be taken from the calculated parking demand for a certain percentage of the parking supply present along a project's lineal frontage. This credit is available for all land use types. The frontage where there are parking meters cannot be included in the frontage available for a parking credit.

8. Service and Loading

Requirements – the minimum number of loading spaces below are specific to the Village Core District. The small-scale retail sales and services uses and the type of office uses require fewer and smaller service areas and loading spaces.

- a. Residential – No spaces; however, residential developments with 12 or more dwelling units shall provide a designated service or loading area that can accommodate moving vans and appliance delivery vehicles.
- b. Office/Retail - 1 van space / 10,000-100,000 square feet. Additional space for each additional 200,000 square feet.
- c. Restaurant - 1 truck space / 80 seats.

Van Loading Space – 14 feet wide by 20 feet long.

Truck Loading Space – 12 feet wide by 35 feet long.

- No portion of any front yard, or any side yard on the street side of a corner lot, shall be used for loading or service purposes.
- Fractional loading space requirements shall be rounded downward.
- No part of an alley or street shall be used for loading, except areas designated by the City for loading or as approved by a Minor Use Permit.

9. Street Elements

Sidewalk use for outdoor seating or displays shall utilize no more than 50 percent of usable

sidewalk space and leave a minimum of 5 feet of unrestricted space for pedestrian circulation. Portions of the landscaped areas used for street tree planting within the public right-of-way may be paved and used for outdoor seating.

10. Landscape

Street trees in public right-of-way and public parking areas. Landscaping in the public right-of-way and public parking areas is intended to create a distinct outdoor character. The medians, diagonal parking and sidewalk between Broadway and the buildings along both sides of Broadway provide the opportunity for street and parking lot tree landscaping that provide shade and relief from the large amount of paving. The existing public parking area south of Lester Avenue and the planned public parking area north of Golden Street must be landscaped to provide shade and break up the large area of paving. Street tree selection and any proposed substitutions to the recommended trees shall consider site and maintenance criteria such as planter area, sidewalk location, root damage potential, water and soil requirements, litter, hazards, longevity, and on-street parking location. A limited variety of trees are allowed to unify the character of the landscaping theme.

- Trees located in the diagonal parking spaces and sidewalk within the right-of-way along Broadway shall be smaller in size with a broad canopy to provide shade and relief from the large amount of paving. Trees shall be a minimum size of a 24-inch box and shall be grouped in designated planting areas at the rate of one for every 40 feet of frontage. They shall be placed in planting areas a minimum of 4 feet x 6 feet. These trees shall include:
 - Agonis flexuosa/Peppermint Tree
 - Lagerstroemia indica, Crape Myrtle (mildew resistant)
 - Prunus cerasifera/Purple-leaf Flowering Plum
 - Pyrus kawakamii/Evergreen Pear
 - Other similar trees may be allowed with the approval of the Director.



- Trees located in the medians within the right-of-way along Broadway shall be more vertical with a high branching structure that will provide shade without blocking the visibility of storefronts or parking areas. These trees shall include:

- Arbutus menziesii/Madrone
- Hymenosporum Flavum/Sweet Shade
- Pinus canariensis/Canary Island
- Pine Tristania conferta/Brisbane Box

- Trees located within the right-of-way along Lester Avenue, Grove Street, Kempf Street and Golden Street shall have a more vertical characteristic to soften the multi story buildings within the Village Core District from adjacent development. Trees shall be a minimum size of a 24-inch box and shall be planted at the rate of one for every 25 feet of frontage located behind curb in tree wells a minimum of 4' x 4'. Tree wells shall be covered with tree grates approved by the City Council. These trees shall include:

- Bauhinea purpurea/ Purple Orchid Tree
- Bauhinea v. candida/ White Orchid Tree
- Hymenosporum flavum/Sweet Shade
- Podocarpus macrophyllus/Yew Pine
- Pinus canariensis/Canary Island Pine
- Tristania conferta/Brisbane box

Other similar trees may be allowed with the approval of the Director.

Landscaping on private property. In any property development, an area creating comfortable and attractive outdoor areas shall include landscaping.

- All landscaping shall be shown on a detailed landscape plan approved by the Director.
- All planting areas shall be served by permanent irrigation systems.

11. Enclosures/Screening

- The screening for parking lots shall include at least four feet (4') of landscaped

strip between the vehicle area and the sidewalk with a 42" high screening element of either a dense hedge or a wall or fence integrated with shrub planting.

- All service areas and trash containers shall be screened from view through the use of dense plant materials and masonry screens or similar solid structures compatible with the building streetscape design.

12. Signs

- Signs in the transom area shall not exceed 65 percent of the width of the transom panel or 75 percent of the height of the transom panel.
- The height of letters shall not exceed 18 inches.
- Projecting signs shall be mounted on the structural piers of the building. They shall be no lower than 8'6" and no higher than the base of the building, or 14 feet, whichever is lower.
- Signs are not allowed on rooftops or projecting above the roof, eave, or parapet.
- Temporary advertising signs, sale signs, lease signs, etc. attached to the building and display windows shall be limited to 30 days of display time.

13. Nonconforming Use/Lot

Pursuant to Section 17.24.090 of the Municipal Code.

14. Temporary Use

Pursuant to Section 17.28.040 of the Municipal Code.

ii. Retail Manufacturing Zone

This zone is intended to create a transitional area between the Village Commercial Zone and the light industrial uses to the north. The



manufacture, assembly or providing of commercial services is allowed and the on-site retail sale of the products produced on the site is encouraged. Retail sale of similar or complimentary or accessory products is also allowed. See Figure 4.

Permitted Uses

In the Retail Manufacturing zone, the following uses are permitted, subject to the Development Standards in this Section and elsewhere in the Downtown Village Specific Plan.

1. Retail sales establishments offering hardware, furniture, electronics, breweries, wineries. Manufacturing, processing or treating of products, which are not obnoxious or offensive by reason of emission of odor, dust, smoke, gas noise, vibration or other causes, or hazardous by way of materials, process, product or other method, and which conform to the performance standards as specified in Section 17.24.080 of the Municipal Code.
2. Retail nursery and greenhouses.
3. Low traffic generating retail sales and services activities including florist or nursery, art supply or hobby store, photography studio, galleries, art studios, custom shop for curtains, draperies, floor coverings, upholstery and wearing apparel.
4. Catering services.
5. Business, professional and technical offices. (e.g., doctor, lawyer, architect, engineer, realtor, travel agent, accountant, insurance or security brokers, advertising agencies, contractors without outdoor storage).

Uses Requiring Discretionary Use Permits

Minor Use Permit

The following uses are permitted subject to the approval of a Minor Use Permit according to the provisions of Section 17.28.052 of the Municipal Code:

1. Telecommunication and cellular telephone communications facilities.

Conditional Use Permit

The following uses are permitted subject to the approval of a Conditional Use Permit according to the provisions of Section 17.28.050 of the Municipal Code:

1. Public service, utility structures and facilities.

Development Standards – Retail Manufacturing Zone

This section establishes site development standards, land use regulations, and other general requirements that apply to all projects in the Retail Manufacturing Zone. Any standards not specifically covered by this Specific Plan are subject to the regulations of the Municipal Code.

1. Minimum Lot Area

10,000 square feet

2. Minimum lot width and depth

Width – 70 feet

Depth –120 feet

3. Building Setbacks

■ North Avenue-south side

Building shall be located within one foot of the ultimate right-of-way.

■ Lester Avenue-north side

Building shall be located within one foot of the ultimate right-of-way.

■ Grove Street-west side

Building shall be located within 10 feet of the ultimate right-of-way.

The intent is the creation of a strong street edge and the removal of rear buildings.

4. Maximum Building Height

45 feet.



5. Minimum Usable Open Space

None required.

6. Off-Street Parking

See previous Off-Street Parking Standards section provided under the Village Commercial Zone. Table B provides standards for the entire District.

7. Service and Loading

The manufacturing, assembly and commercial services uses provided in this zone require the frequent use of trucks.

A minimum of one truck loading space shall be required per lot or business.

Truck Loading Space: 12 feet wide by 35 feet long.

- No portion of any front yard, or any side yard on the street side of a corner lot, shall be used for off-street loading purposes.
- Fractional loading space requirements shall be counted as a whole space.
- No part of an alley or street shall be used for loading, excepting areas designated by the City for loading in a Minor Use Permit.
- Requirements for uses not specifically listed herein shall be based upon the requirements for comparable uses listed and upon the particular characteristics of the use as determined by the Planning Director.
- Service access and alleys shall be visually minimized from Broadway and shall not be accessed directly from Broadway.

8. Street Elements

Commercial sidewalks along the public streets within this zone shall not accommodate outdoor spaces for seating or displays.

9. Landscape

Street trees in public right-of-way- Landscaping in the public right-of-way is intended to create a distinct outdoor character. The sidewalk between streets and the buildings provide the opportunity for street tree landscaping that provide shade and screening of this zone from adjacent properties. Lester and Grove Streets must be landscaped to provide screening. Street tree selection and any proposed substitutions to the recommended trees shall consider site and maintenance criteria such as planter area, sidewalk location, root damage potential, water and soil requirements, litter, hazards, longevity, and on-street parking location. A limited variety of trees are allowed to unify the character of the landscaping theme.

- Trees located within the right-of-way along Lester Avenue, Grove Street, shall have a more vertical characteristic to soften the multi-story buildings within the Retail Manufacturing Zone from adjacent development. Trees shall be a minimum size of a 24-inch box and shall be planted at the rate of one for every 25 feet of frontage located behind curb in tree wells a minimum of 4'x 4'. Tree wells shall be covered with tree grates approved by the City Council. These trees shall include:
 - Bauhinia purpurea/Purple Orchid Tree
 - Bauhinia v. candida/White Orchid Tree
 - Hymenosporum flavum/Sweet Shade
 - Podocarpus macrophyllus, Yew Pine
 - Pinus canariensis, Canary Island Pine
 - Tristania conferta/Brisbane Box

Other similar trees may be allowed with the approval of the Director.

10. Enclosures/Screening

- The screening for parking lots shall include at least four feet (4') of landscaped strip between the vehicle area and the sidewalk with a 30" high screening element of either a dense hedge or a wall or fence integrated with shrub planting.



11. Signs

- Signs in the transom area shall not exceed 65 percent of the width of the panel or 75 percent of the height of the panel.
- The height of letters shall not exceed 18 inches.
- Projecting signs shall be mounted on the structural piers of the building. They shall be no lower than 8'6" and no higher than the base of the building, or 14 feet, whichever is lower.
- Temporary advertising signs, sale signs, lease signs, etc. attached to the building and display windows shall be limited to 30 days of display time.

12. Nonconforming Use/Lot

Pursuant to Section 17.24.090 of the Municipal Code.

13. Temporary Use

Pursuant to Section 17.28.040 of the Municipal Code.

d. Design Guidelines – Village Core

The intent of the Village Core Design Guidelines is to preserve and enhance the unique “main street” character of Broadway between Lemon Grove Avenue and Grove Street. Broadway is the main east-west thoroughfare through Downtown Lemon Grove and is seen as a key component of the City’s commercial identity.

Design guidelines are also presented for the surrounding streets such as Lester Avenue, Grove Street and Golden Avenue.

i. Site Planning and Street Design

Street design guidelines for the Village Core preserve and enhance the existing character of Broadway as described above.

The character shall be typified by streetscape spaces that are pedestrian-oriented with strong pedestrian linkages between adjacent neighborhoods, parking lots and public transportation. Design guidelines include:

Broadway Corridor

- At the street level, buildings shall maintain a uniform setback, as described in earlier sections of this chapter, to maintain the continuity of the “street wall.”
- The identity of Broadway shall be strengthened by a pedestrian-oriented streetscape including sidewalks and street lighting theme.
- Where feasible, replace selected parking spaces to create outdoor public spaces for shared restaurant seating along the diagonal parking spaces on the north and south sidewalk edges along Broadway. See figures 4.1 and 4.2.
- Buildings shall be organized with primary entries oriented towards Broadway.
- Where feasible, provide pedestrian paseo linkages between buildings that link (see public open space design guidelines in this section).
- Improve secondary access to buildings by providing secondary building entries from paseos, rear parking lots, and from side street building elevations.
- At the street level, buildings shall have pedestrian-oriented uses on the ground floor.

Lester Avenue Corridor

With the relatively recent public improvements, which include the construction of a new public parking lot, sidewalk/landscape treatments along sections of its southern edge, and having a prominent entry to an existing pedestrian paseo that leads to the Broadway shops, Lester Avenue has the potential to develop its own unique



identity within the Village Core District. The streetscape guidelines presented below encourage the continuation of the development of a pedestrian-friendly walking environment as well as providing off-site parking improvements.

- The identity of Lester Avenue shall be reinforced by a pedestrian oriented streetscape including wider sidewalks, street lighting theme and a landscaped parkway with street trees.
- Recent sidewalk improvements along a portion of the southern edge of Lester Avenue (at edge of parking lot) shall be extended in both directions along the entire corridor.
- Buildings shall be organized in orthogonal blocks, perpendicular to the street with primary entries oriented towards Lester Avenue.
- On-site parking shall be located behind or to the sides of buildings, preferably in shared lots (see parking guidelines this section).

ii. Storefront Design

Design guidelines for Village Core storefronts provide criteria that promote quality design for the ground floor of new buildings and rehabilitation of existing buildings. Guidelines encourage and support the mix of traditional and modern building facades found within this District. The criteria presented below are intended to supplement the existing May 2001 Downtown Storefront Improvement Guidelines provided under a separate document. Additional guidelines include:

- The storefront is defined as the part of the building that fills the structural bay on the front facade at ground level. Storefronts shall face principal streets, including Broadway and Lester Avenue. If a building is located on a corner lot, storefronts may be on both street faces.
- Storefronts within this district shall typically

consist of a base (the area under the main window), a main transparent window, a transom above the window, and a storefront cornice.

- Storefront design, materials and colors, of both existing and new buildings, shall compliment the individual architectural character of each building.
- Large openings filled by windows and attractive window displays create a welcome ambience in this retail district and add interest to the pedestrian experience. At least 60 percent of the total area of the first floor façade facing Broadway shall be window display.
- Walls exposed to side streets, such as Lemon Grove Avenue or Grove Street, or pedestrian paseos must have doors and/or windows, or fixed glazing areas of at least 30 percent of the total surface area of the wall facing these open spaces.
- At the street level, use of storefront transoms is encouraged. Transoms can assist with diffusing direct daylight into a space, as well as provide operable openings to allow natural ventilation.
- No visible exterior security gates or bars are permitted. Interior security systems or enclosures shall not be visible from the exterior of the building during normal business hours.
- Avoid flat facades by using recessed or projected entryways, bays, canopies, awnings and other architectural elements.
- Building bases are an important element of the storefront design. Materials shall compliment the character and variety of architecture found within this District. Materials may include aluminum/glass storefront systems, stucco, tile, wood, and/or masonry.



iii. Landscape/Hardscape

Landscape guidelines, incorporating both plants and paving, are intended to encourage the creation of comfortable and attractive outdoor spaces and provide directional assistance to pedestrians. This is accomplished by using street trees to either enclose, screen, shade or direct attention to the outdoor space. The use of a variety of paving materials and low planting areas create transitional zones between the public areas of the street and parking areas to assist motorists and pedestrians in finding their way between parking spaces and building entrances. See figure 4.3.

Landscape

- Streetscape planting should reflect a consistent alignment of trees with shrubs and groundcover plantings to enhance the visual experience.
- Plantings near buildings and in courtyards, terraces, plazas, and other outdoor spaces for human activity should include trees with canopy branches that create shade. Additionally planting schemes that provide visual variety with form, texture and color are encouraged.
- Streetscape planting should screen and provide a visual buffer between the Retail Manufacturing Zone and adjacent properties.

Hardscape

- At the intersections of Broadway and Lemon Grove Avenue and Broadway and Grove Street, crosswalks shall utilize enhanced paving materials at the street surface to emphasize these intersections as gateways and plaza in the Village Core District
- Mid-block crosswalks shall be introduced on Broadway to increase public safety as well as to reinforce a pedestrian friendly walking environment. Enhanced paving materials

shall match those found at crosswalks described above.

- Paving materials should be used to assist motorists and pedestrians in finding the way between parking spaces and building entrances.

iv. Signage and Identity

Signage and identity design guidelines for the Village Core have been developed to strengthen the District's specific character and identity within the City of Lemon Grove.

The intention of these design guidelines is to:

- Identify Broadway as the primary commercial corridor within the Downtown Village.
- Emphasize a “downtown center” by providing a freestanding monument sign at the Broadway center median, located mid-block between Lemon Grove Avenue and Grove Street. (The intent is to help draw pedestrian traffic to the Village Core area from the trolley station and nearby transit stops).
- Encourage and support pedestrian activity along Broadway by providing consistent, easy to read storefront signage.
- Compliment the variety of architectural styles found along Broadway, but still create a sense of design consistency.

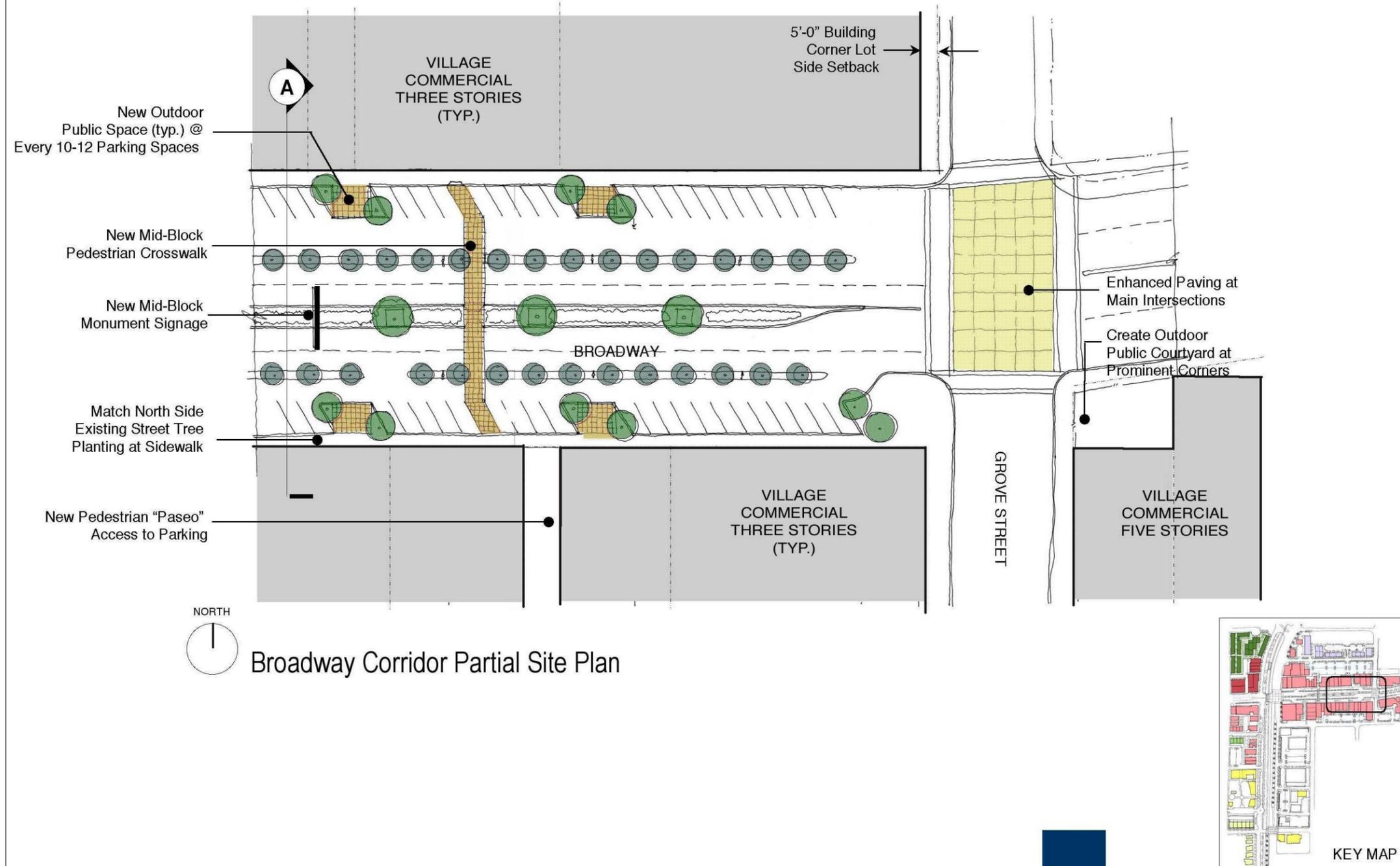
In addition to these standards, commercial signs are regulated by the City of Lemon Grove Planned Development Ordinance requirements.



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Village Core



City of Lemon Grove Downtown Village Specific Plan

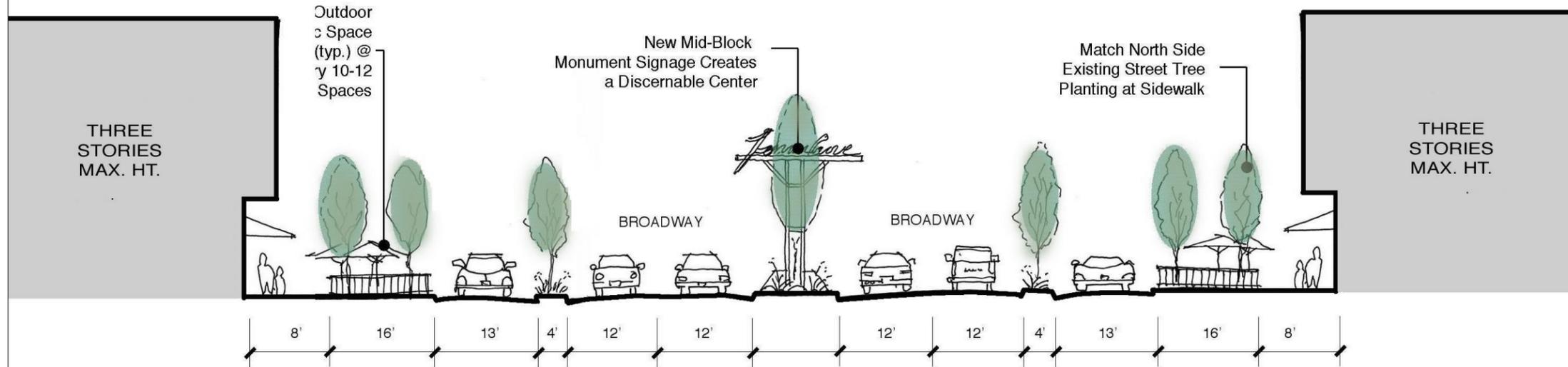


Roesling Nakamura Architects, Inc.

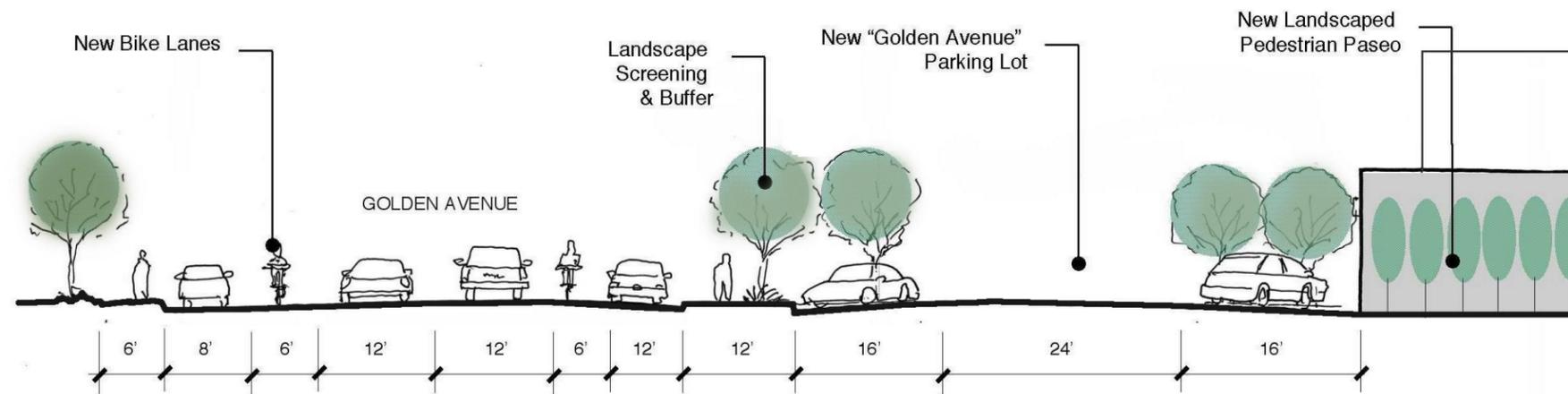
Figure 4.1



Village Core



Section A - Broadway



Section B - Golden Avenue

No Scale



City of Lemon Grove Downtown Village Specific Plan



Roesling Nakamura Architects, Inc.

Figure 4.2

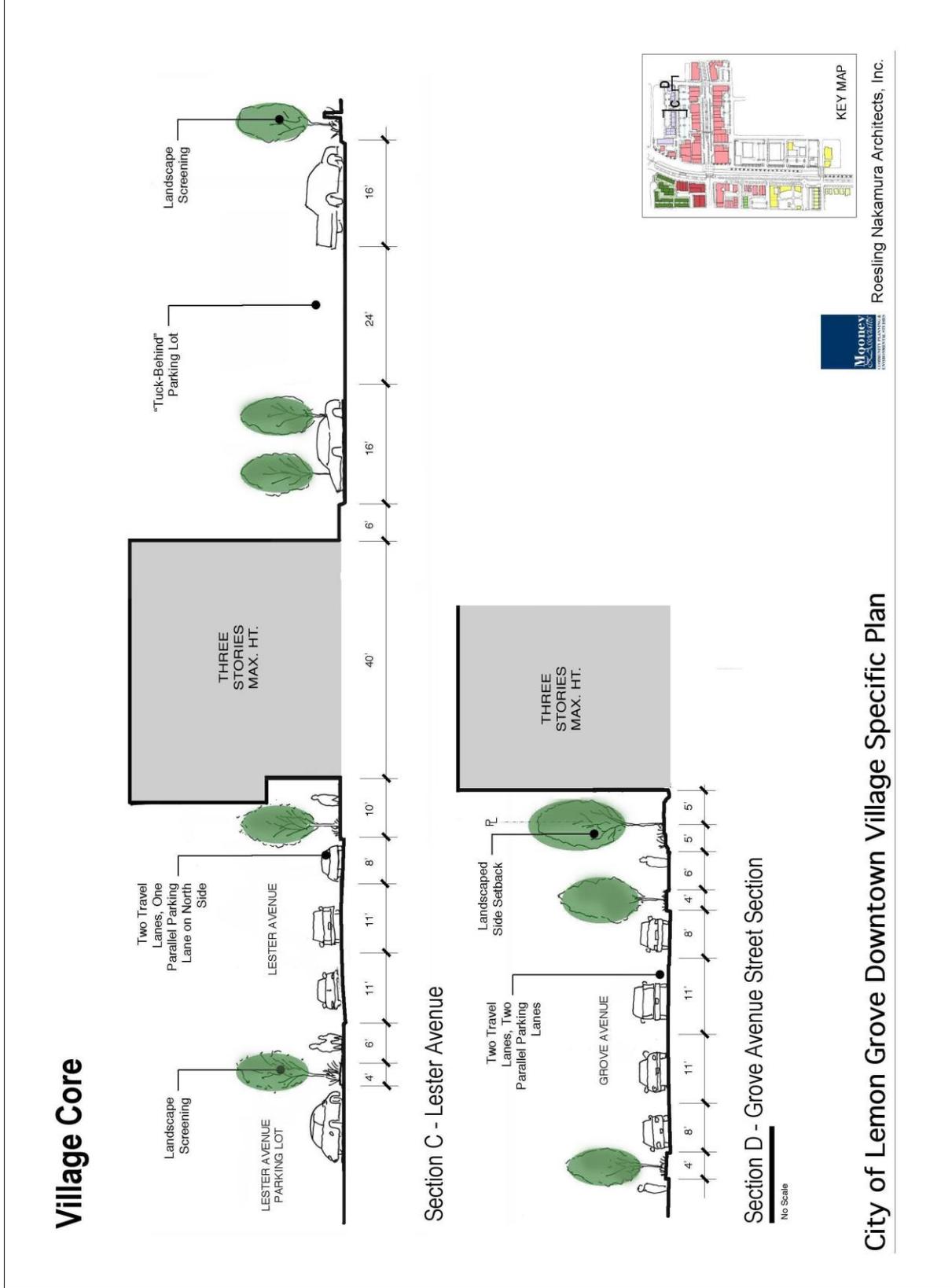


Figure 4.3



Monument and Gateway Signage

A monument sign can help to establish Downtown’s “center” and/or identify primary and secondary gateways into the overall study area. Design guidelines include:

- Provide a freestanding monument sign at the Broadway center median, located mid-block between Lemon Grove Avenue and Grove Street. See Figure 4.4.

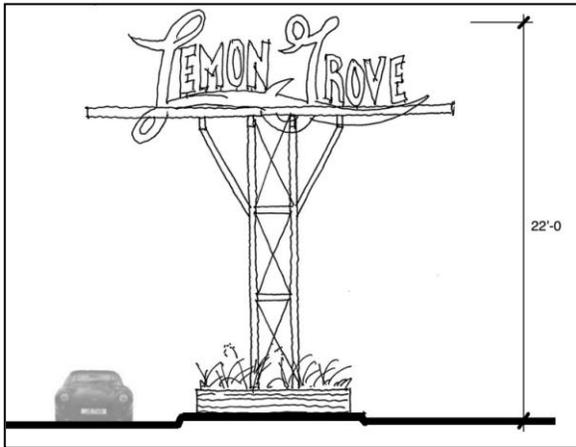


Figure 4.4 Monument Gateway Signage

- Combine the monument sign element with a proposed mid-block pedestrian crossing area (see Landscape / Public Space Guidelines).
- A new freestanding monument sign shall support the “Main Street” character of Downtown Lemon Grove by utilizing traditional materials, colors and text. The sign shall compliment the City’s logo. Traditional materials include wood or steel framing with exposed connections and fasteners.
- A monument sign shall be visible from key intersections and pedestrian plazas along the Broadway corridor.

Exterior Building Signs

The base of the building is the most visible part of the building both to pedestrians and motorists. To be effective, signs must call attention to the business and create an individual image for the

store. Signs also contribute to the overall image of a district, adding variety and liveliness to both building facades and streetscapes.

The theme of the Village Core encourages both traditional and modern storefront signage with the intent to strengthen the pedestrian experience and communicate a high-quality businesses identity. Signage guidelines are:

General Criteria

- Signs are typically intended to be viewed by motorists and pedestrians and shall be oriented to establish the location, identity, and character of the use of the retail establishment.
- Signs for businesses shall only include the formal name of the business, the nature of the business and the address. There shall be no advertising of brand names.
- To encourage diversity in design, there are no specific fonts or letter style that are recommended; however, a letter style shall be chosen that is easy to read and that represents the image of the business it is presenting.
- Signs are not allowed on rooftops or projecting above the roof, eave, or parapet.
- No flashing, or moving signs are permitted.
- Temporary signs in windows are not allowed except under limited conditions for short periods of time.

Building Facade Wall Signs

- Opaque signs or panels shall not cover the transom areas above the door or display windows. If the transom is divided into a number of smaller panels, the sign location shall not obscure the individual panels.
- Signs in the transom area shall not exceed 65 percent of the width of the panel or 75 percent of the height of the panel.



- The height of letters shall not exceed 18 inches.
- Automobile-oriented external wall signs shall not exceed fifty (50) square feet. Sign types shall fit within the architectural details of the building they are placed on.

Pedestrian Oriented Signage

- Pedestrian-level display window signs applied directly to the glass shall consist of individual lettering without an opaque background. On the display window, such a sign shall not cover more than 20 percent of the area of the glass panel.
- Window signs shall not obscure the display area. The color of letters shall contrast with the display background. Light colored letters or gold leafed letters with dark borders are effective.
- Hanging signs shall project no more than three feet from the face of the building. Brackets and other methods of attachment shall be considered part of the design proposal and shall be designed to be compatible with the building.
- Window signs shall not exceed 40 percent of the window surface area. For shops and businesses along Broadway, smaller pedestrian-oriented scaled window signs are encouraged.

Projecting Signs

- Projecting signs shall be mounted on the structural piers of the building. They shall be no lower than 8'6" and no higher than the base of the building or 14 feet, whichever is lower.
- Projection signs shall compliment the architecture of a building in respect to colors, materials, and structure. Only one projection sign shall be allowed per building. See Figure 4.5.

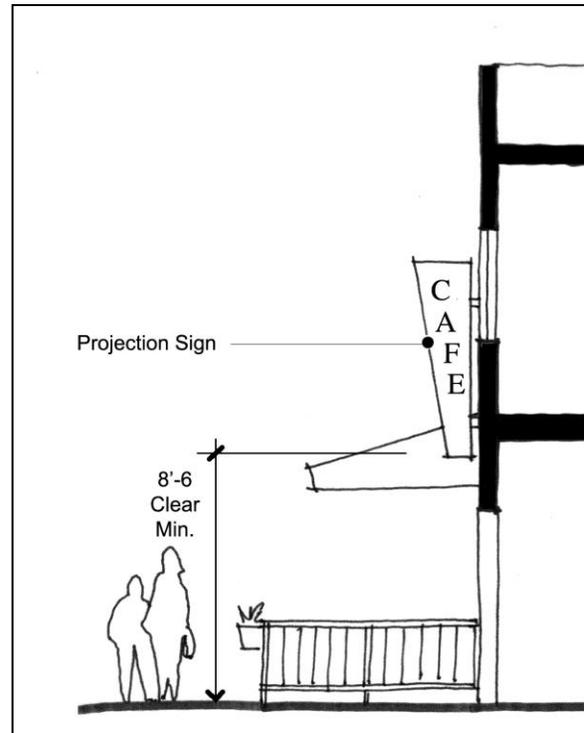


Figure 4.5 Projecting Sign

- All projecting signs may be externally lit. External light sources shall be shielded from viewers on the street to prevent glare. No flashing, or moving signs are permitted.

Awnings

- Awnings or canopies shall be designed consistent with the door and window openings or structural bays of the building façade.
- To encourage a pedestrian scale along Broadway, street-level awnings shall provide coverage for the storefront area only including transom spaces. Large multi-story awnings are discouraged.
- Awnings can be constructed of several materials including fabric, steel, metal, and wood.
- Fabric awnings shall be made of canvas or Neoprene impregnated fabric. Vinyl awnings are not permitted. The underside of awnings shall be open to expose the structure.

- Signs on awnings shall have contrasting letters painted or sewn onto the valance or skirt of the awning. Six to eight inch letters are recommended.
- Letters on the main, sloping area of the awning are not permitted.

v. Public Spaces

The Village Core District is dominated by Broadway, which is a major circulation corridor, that provides through vehicular circulation as well as vehicular access to public parking areas in front of the Village Commercial land uses and pedestrian access to the Village Commercial uses. The street and parking medians, mid-block pedestrian crossing, small plazas, paseos, storefronts and outdoor uses in the public right-of-way, especially along Broadway, will become significant elements of the urban landscape.

The parking areas located behind the Village Commercial land uses should be well landscaped and screened without making them hard to find. Directional signage to direct cars to additional public parking behind the Village Commercial land uses and to direct pedestrians from the parking areas to the retail commercial uses is an important element of this District.

vi. Streetscape Elements

Streetscape elements shall be the unifying feature of all four Downtown Districts. Streetscape elements shall compliment the traditional “Main Street” theme and items shall include; tree grates, benches, directional signage, street name signage, freestanding information kiosks, trash cans, lighting fixtures, bus stops shelters and benches.

- Coordinate the design and placement of high-quality streetscape elements throughout the District.
- Adopt a public art program to strengthen the cultural and/or historical identity of Lemon Grove. Public art sites shall be located within pedestrian plazas, landmark locations, and at District entries. Location opportunities for art include: center

median, pedestrian paseos, and at major intersections.

- Install wayfinding and informational signs to increase public understanding of the District.
- All service areas and trash containers must be screened from view through the use of dense evergreen plant materials, and decorative masonry screens or similar solid structures compatible with the building streetscape design.
- Landscaping and on-street parking along sidewalks shall provide a sense of spatial enclosure, which typically encourages pedestrian activity.
- Appropriate landscape buffers shall be designed and maintained in parking areas to enhance the external view of the parking area and to screen the parking from the adjacent residences. Landscape buffers can include landscaping or hardscape elements.

Pedestrian “Paseos”

Where feasible at existing and planned open spaces between buildings that link to parking areas or adjacent neighborhoods, create pedestrian passageways, also known as “Paseos.” Paseos allow property and business owners to increase their façade exposures to the public while providing safe pedestrian-scaled passageways between parking areas to the building frontages. See figure 4.6. Design criteria include the following:

- Paseos shall be proportionate to their length and shall be no less than 8’-0” in width.
- Building facades facing to paseos shall have windows or fixed glazing areas of at least 30 percent of the total surface area of the wall facing the paseo.
- Paseos shall include landscape planters, outdoor public seating spaces, site walls and lighting.



- Paseos may also include shade structures, such as fabric structures or lattice systems to promote a pedestrian scaled environment and provide shade spaces and cover during inclement weather.
- Secondary building entries off of a paseo shall have a projection and/or hanging identity sign that should project no more than three feet from the face of the building. Brackets and other methods of attachment shall be considered part of the design proposal and shall be designed to be sensitively compatible with the building.

Lighting

Pedestrian-scaled lighting at the public right-of-way and at commercial property shall compliment the existing lamp fixtures along Broadway and the Lester Avenue parking lot. (Figure 3.15)

Banners

Banners on light poles is an effective way of adding visual interest and drawing attention to special civic events within Downtown Lemon Grove; however, to be effective, banners must be maintained at regular intervals to reflect seasonal changes or announce special events.



Before



After

Figure 4.6 Paseos



vii. Parking

Parking and circulation guidelines are provided to enhance public access to commercial buildings as well as provide clear and easy circulation, parking and access to principle commercial streets. Parking shall be convenient for customers and visitors and their visual impact shall be softened or minimized. Specific guidelines include:

- Except for the existing diagonal frontage road street parking along Broadway, on-site parking shall be located behind or to the sides of buildings. (Figure 3.16)
- Access to rear lot parking shall be from streets other than Broadway.
- It is encouraged to provide a single, shared, rear-lot parking area located behind the southern row of commercial buildings along Broadway. The design intent is to consolidate existing rear lot parking spaces into a single public parking area similar to the Lester Avenue Parking lot.
- Provide clear, accessible and safe pedestrian landscaped pathways from the parking areas to principle commercial streets. Pathways shall have a minimum clear width of 60” and shall meet State and National accessibility requirements. Surface materials shall include concrete or other durable materials, such as cast-concrete pavers and/or brick.
- All parking lots shall incorporate landscaping and have a concrete curbed perimeter. Islands shall provide shade trees in the interior of the parking lot and there shall be a minimum of one shade tree per island; two trees per island are required where opposite bays align. Trees within islands shall be aligned between parking spaces.
- On-site parking shall be located behind or to the sides of buildings, preferably in municipal lots. Since this is not realistic in

the short-term, the lots could initially be private and some side parking lots can be permitted, as long as they are landscaped in a way that provides a strong street edge compatible with long-term development. The side parking lots could eventually be replaced with buildings along the frontage, particularly as buildings in the rear portions of the lots are demolished over time and replaced with rear parking lots.

- Provide landscape buffers or screening elements that separate public right-of-ways from parking and to help soften the visual impact of parking lots edges.
- Provide single shared parking lot accessible off Golden Avenue located behind the commercial buildings on the southern portion of Broadway.
- Buffers or screens shall be composed of a minimum four feet (4’) of landscaped strip between the vehicle area and the public sidewalk with a 42” high screening element of either a dense hedge, a wall or fence integrated with shrub planting. Landscape buffers may also include site walls or arcades. See Figure 4.7.



Figure 4.7 Broadway Parking Buffer

- All parking areas shall provide adequate lighting at nighttime utilizing fixtures that support the scale, character and identity of the Village Core District (see lighting standards for specific information).



- Directional signage and/or information kiosks within parking lots are encouraged and shall be visually attractive, complimenting the scale, character and identity of the Village Core District (see signage guidelines for specific information).
- All parking areas shall comply with the City of Lemon Grove Planned Development Ordinance requirements and with State and Federal accessibility requirements.



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3 – CIVIC CENTER

a. Purpose

The Civic Center District is intended to accommodate the various governmental services and public amenities used by the citizens of Lemon Grove. The District shall be easily accessible to the community, with the inclusion of generous landscaped open spaces available for public use. This District shall also continue the improvements along Main Street such as diagonal parking, landscape and hardscape treatments. Medium density residential provides a transition to neighboring lower density residential areas to the south and west.

Development Standards in this district promote attractive, subtle, non-monumental civic structures and a pedestrian-friendly character, including well-designed parking with efficient internal access, ingress and egress, and amenities such as plazas, and attractive streetscapes.

b. Land Use Descriptions

These land uses are intended to provide for existing and future civic uses and to preserve the existing multi-family residential uses (Figure 3).

i. Civic: This land use allows for public uses and service facilities, such as government offices and facilities, public utilities, post office, libraries, and fire and law enforcement stations.

ii. Medium/High Density Residential: Duplexes, town houses, condominiums and apartments.

iii. Mixed Use: Mix of residential (condominiums and apartments), retail and office uses within the same building, lot or area, with the intent of creating lively pedestrian-oriented villages near the trolley stations.

iv. Retail Commercial: Retail operations providing a broad range of goods and services, catering to both local and regional customers.

c. Zones, Uses & Development Standards

i. Civic Zone

This zone is intended to provide an area for uses and services typically provided to residents by the City. This includes uses and services such as government offices, public utilities, post office, libraries, fire and law enforcement stations, and social services facilities. See Figure 5.

Permitted Uses

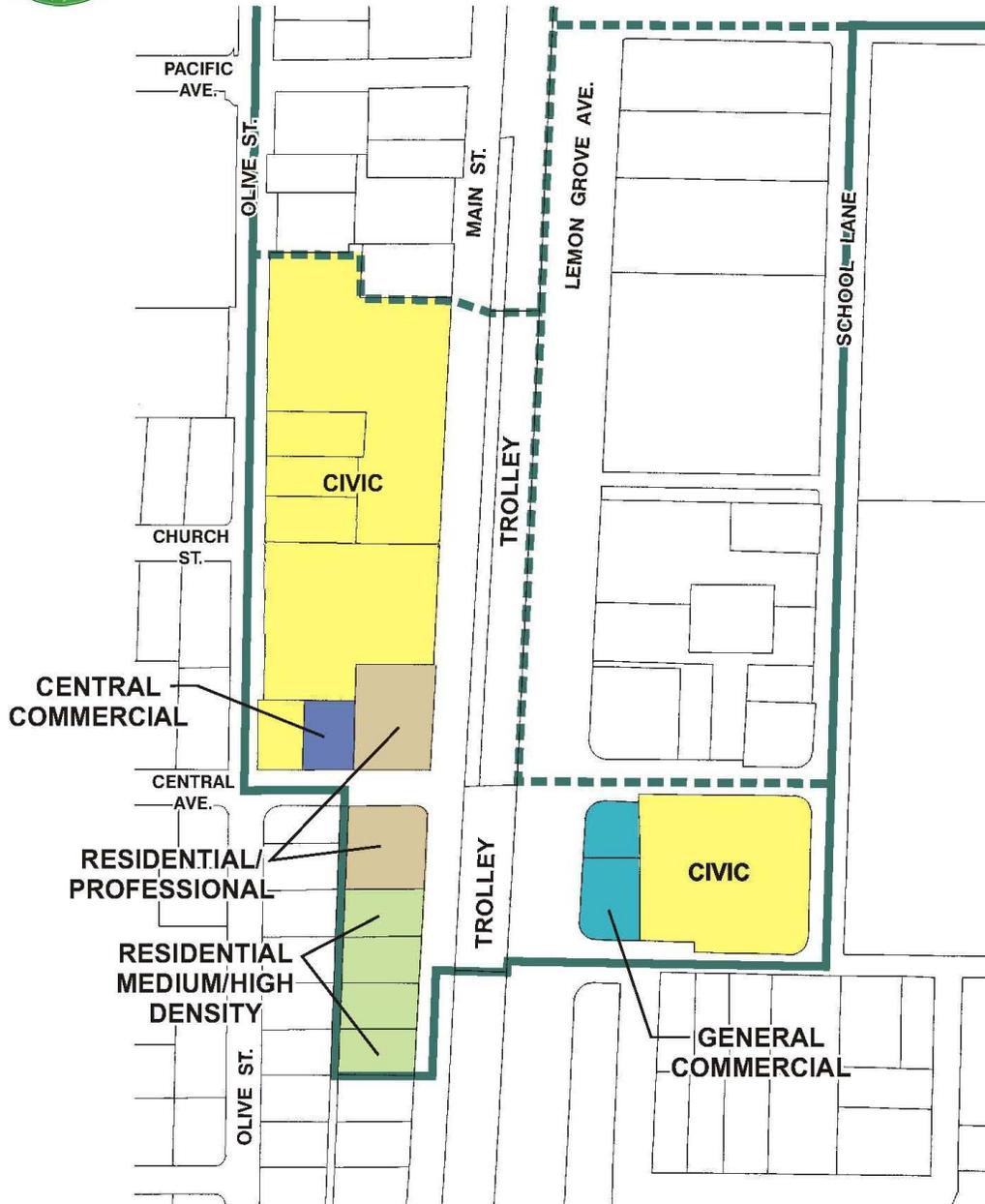
In the Civic zone, the following uses are permitted, subject to the Development Standards in this Section and elsewhere in the STA 1 ordinance specified.

1. Government Offices and Facilities.
2. Cultural Activities (e.g., library, museum, gallery).
3. Fire and Law Enforcement Stations.
4. Social Service Facilities.
5. Community Centers.
6. Parks.
7. Any other business or service establishment which the Planning Director finds to be consistent with the purposes of this Specific Plan and the intent of the General Plan.





Civic Center Zones



CIVIC CENTER ZONES
Figure 5



Uses Requiring Discretionary Use Permits

Minor Use Permit

The following uses are permitted subject to the approval of a Minor Use Permit according to the provisions of Section 17.28.052 of the Municipal Code:

- 1. Telecommunication and cellular telephone communications facilities.

Conditional Use Permit

The following uses are permitted subject to the approval of a Conditional Use Permit according to the provisions of Section 17.28.050 of the Municipal Code:

None.

Development Standards – Civic Zone

This section establishes site development standards, land use regulations, and other general requirements that apply to all projects in the Civic Zone. Any standards not specifically covered by this Specific Plan are subject to the regulations of the Municipal Code.

1. Minimum Lot Area

No minimum lot area requirement

2. Minimum lot width and depth

No minimum lot width or depth requirement

3. Building Setbacks

- **Main Street – west side**
Buildings shall be located no closer than 10 feet from the ultimate right-of-way.

- **Olive Street – east side**
Buildings shall be located no closer than 10 feet from the ultimate right-of-way.
- **Central Avenue**
Buildings shall be located no closer than 10 feet from the ultimate right-of-way.
- **Lemon Grove Avenue – east side**
Buildings shall be located no closer than 10 feet from the ultimate right-of-way.
- **Lincoln Street – north side**
Buildings shall be located no closer than 10 feet from the ultimate right-of-way.

4. Maximum Building Height

45 feet

5. Mixed Use within same building on same lot

n/a

6. Minimum Usable Open Space

n/a

7. Off-Street Parking

Table C shows the parking calculations for the Civic Center District. This District is calculated to need 875 parking spaces. This requirement is based on the factors discussed below. The basic parking rate is based on the size of the building and/or the number of dwelling units and includes a reduction for transit. Additional reductions are for the variety of civic uses of the area and for shared parking. This table also assumes an average of two bedroom dwelling units, and all residential parking is in designated spaces not available to the public.



**Table C
Civic Center**

| Use | Size | Parking Rate | Gross Parking Demand | Mixed-Use Reduction | Shared Parking Reduction @ Noon | Net Parking Demand |
|--|------------|--------------|----------------------|---------------------|---------------------------------|--------------------|
| Civic Center | | | | | | |
| -Civic Office | 151,338 SF | 5.0 /KSF | 757 | 1.00 | 1.00 | 757 |
| -Commercial | 22,006 SF | 4.0 /KSF | 88 | 1.00 | 1.00 | 88 |
| -Office | 14,230 SF | 4.0 /KSF | 57 | 1.00 | 1.00 | 57 |
| -Residential ⁽¹⁾ | -19 du | 2.00 /du | 38 | 1.00 | 1.00 ⁽²⁾ | 38 |
| | | | 940 | | | 940 |
| Grand Total | | | 940 spaces | | | 940 spaces |
| Notes: | | | | | | |
| (1) Assume average of two bedroom units. | | | | | | |
| (2) Assume residential has designated parking not available for sharing. | | | | | | |
| SF = square feet (gross) | | | | | | |
| KSF = 1,000 square feet | | | | | | |
| du = dwelling unit | | | | | | |

Parking Rates - The minimum parking rates listed below are specific to the Civic Center District. They reflect reductions from standard suburban rates due to the accessible public transit available in the immediate area. Additional reductions for mixed uses and shared parking are possible.

- a. Residential (spaces per dwelling unit)
 - i. Studio – 1.0
 - ii. 1 bedroom – 1.25
 - iii. 2 bedroom – 1.75
 - iv. 3 bedroom – 2.0
- b. Office - 3.0 spaces / 1,000 square feet

Mixed-Use Reduction- The mixed-use nature of the area and potential redevelopment suggest that a mixed-use parking rate reduction is not warranted.

Shared Parking- This is not applicable in the Civic Center District with the proposed civic office and residential land uses.

In-lieu fees- This is an opportunity for redevelopment projects to purchase off-site parking when on-site parking cannot reasonably be provided. It is usually applicable in small or narrow lot situations, where providing on-site parking would remove the building that is generating the parking need and/or the lot is too

small for the physical maneuvering of cars. New projects shall be required to provide Code required parking on site, but there are situations where it is more efficient for several new projects to provide a single common lot than smaller individual parking lots.

Up to 100 percent of the parking requirement could be satisfied through the payment of an in-lieu fee to be established by the City Council.

On-Street Parking- Credit for on-street parking can be taken from the calculated parking demand for a certain percentage of the parking supply present along a project's lineal frontage. This credit is available for all land use types. The frontage where there are parking meters cannot be included in the frontage available for a parking credit.

8. Service and Loading

The variety of public uses and services provided in this zone requires a variety of service areas and loading spaces. The individual service and loading requirements for each civic use shall be considered and specifically reviewed prior to the approval of any project within this District to guarantee that adequate service areas and loading zones are provided.



- No portion of any front yard, or any side yard on the street side of a corner lot, shall be used for off-street loading purposes.
- Fractional loading space requirements shall be counted as a whole space.
- No part of an alley or street shall be used for loading, excepting areas designated by the City for loading in a Minor Use Permit.
- Requirements for uses not specifically listed herein shall be based upon the requirements for comparable uses listed and upon the particular characteristics of the use as determined by the Planning Director.

9. Street Elements

Sidewalk use for outdoor displays or kiosk-type uses shall utilize no more than 50 percent of usable sidewalk space and leave a minimum of 5 feet of unrestricted usable space for pedestrian circulation.

10. Landscape

Street trees in public right-of-way. Landscaping in the public right-of-way is intended to create a distinct outdoor character as well as accentuate the strong pedestrian linkage from the Integrated Transit Station to the Civic Center District. The generous sidewalk dimensions along the west side of Main Street provide the opportunity for larger, wide canopy trees that provide shade and a sense of enclosure. Street tree selection and any proposed substitutions to the recommended trees shall consider site and maintenance criteria such as planter area, sidewalk location, root damage potential, water and soil requirements, litter, hazards, longevity, and on-street parking location. A limited variety of trees are allowed to strengthen the individual character of the Civic Center District while linking it to the Main Street Promenade District.

- The predominant trees located within the public right-of-way along both sides of Main Street shall be large, broad canopy trees intended to create a more formal promenade. Trees shall be a minimum size of a 36-inch

box and shall be planted on 40' centers in planting areas a minimum of 6 wide and 8 feet long. The entire planting area shall be planted in low shrubs or ground cover. The recommended tree along Main Street is *Podocarpus gracillior*, African Fern Pine. Alternative choices are:

- *Plantanus acerifolia*
- *Tipuana tipu*/Tipu Tree
- *Pinus pinea*/Italian Stone Pine, *Koelreuteria bipinnata*/Chinese Lantern

- Trees located within the right-of-way along Olive Street, Central Avenue, east and west of Lemon Grove Avenue, and Lincoln Street shall have a more vertical characteristic to soften the multi-story buildings within the Civic Center District from adjacent development. Trees shall be a minimum size of a 24 inch box and shall be planted at the rate of one for every 25 feet of frontage in planting areas a minimum of 4' x 5'. These trees shall include:

- *Bauhinea purpurea*/Purple Orchid Tree
- *Bauhinea v. candida*/White Orchid Tree
- *Hymenosporum flavum*/Sweet Shade
- *Podocarpus macrophyllus*, Yew Pine
- *Pinus canariensis*, Canary Island Pine
- *Tristania conferta*/Brisbane Box

Other similar trees may be allowed with the approval of the Director.

11. Enclosures/Screening

- All permitted or conditionally permitted activities except parks, but including all accompanying accessory uses, shall be confined to enclosed structures, and shall not be conducted or occur upon any open or unenclosed area, whether private or public unless expressly permitted in this Section or authorized by a Conditional Use Permit or Minor Use Permit.
- The screening for parking lots shall include at least four feet (4') of landscaped strip between the vehicle area and the sidewalk with a 30" high screening element of either a



dense hedge or a wall or fence integrated with shrub planting.

12. Signs

- Exterior signs shall be located on the ground level of the building or on a prominent monument signage located near major public circulation corridors such as Main Street.
- The height of the letters shall not exceed 18 inches.
- Signs are not allowed on rooftops or projecting above the roof, eaves, or parapets.

13. Nonconforming Use/Lot

Pursuant to Section 17.24.090 of the Municipal Code.

14. Temporary Use

Pursuant to Section 17.28.040 of the Municipal Code.

ii. Residential/Professional Zone

Permitted Uses

Refer to Section 17.16.050 A, B, and C of the Development Code for Permitted Uses and Uses requiring Discretionary Use Permits.

Development Standards

Refer to Section 17.16.050 D and E of the Development Code for Development Standards.

iii. Residential Medium/High Density Zone

Permitted Uses

Refer to Section 17.16.040 A, B, and C of the Development Code for Permitted Uses and Uses requiring Discretionary Use Permits.

Development Standards

Refer to Section 17.16.040 D and E of the Development Code for Development Standards.

iv. Central Commercial Zone

Permitted Uses

Refer to Section 17.16.060 A, B, and C of the Development Code for Permitted Uses and Uses requiring Discretionary Use Permits.

Development Standards

Refer to Section 17.16.060 D and E of the Development Code for Development Standards.

v. General Commercial Zone

Permitted Uses

Refer to Section 17.16.070 A, B, and C of the Development Code for Permitted Uses and Uses requiring Discretionary Use Permits.

Development Standards

Refer to Section 17.16.070 D and E of the Development Code for Development Standards.

d. Design Guidelines – Civic Center District

The intent of the Civic Center Design Guidelines is to preserve and enhance the traditional “Main street” character at City of Lemon Grove’s civic center core. Main Street is a key component of the City’s civic, cultural, and commercial identity.

The intent of these Design Guidelines is to:

- a. Strengthen and preserve the existing character of Downtown Lemon Grove by complimenting existing and proposed setbacks, massing, architectural styles and materials present within the Civic Center District along Main Street.
- b. Create a strong visual and physical linkage between the Civic Center District and its adjacent Districts.



- c. Preserve and strengthen the design and identity of civic open spaces and buildings.

i. Site Planning and Street Design

Site planning and street design criteria for the Civic Center District promote buildings, streets and public spaces as vital elements of the community’s civic identity. These elements deserve distinctive form since their roles are different from that of other buildings and places within the overall fabric of the Downtown Village. The identity of the Civic Center District shall reflect Main Street’s pedestrian oriented streetscape environment, including sidewalks, a street lighting theme and landscaped parkway with street trees.

Design Guidelines include:

- Instill a sense of prominence within the community by building features such as primary openings and entries that shall align with significant features of the Civic District. These elements may include items within the adjacent park, other significant civic buildings, prominent public corridors (either pedestrian or auto), or have a main public entry off of a major corridor.
- Create hierarchy of scale of building elements as perceived by the pedestrian from parking areas to walkways to building entrances. Massing for new buildings shall be two-story minimum to create a prominent scale along Main Street.
- At the public right-of-way, both streetscape and sidewalk designs shall have a seamless connection between the Civic Center and Main Street Promenade Districts. Building setbacks along Main Street shall accommodate usable outdoor spaces to promote pedestrian activity.
- Entry(s), walkways and plazas shall have design elements that relate to pedestrian scale and shall be comfortable and inviting.

- Plazas and patios shall provide a variety of interesting outdoor spaces. Public spaces shall be intimate yet have an open and accessible feeling.
- Site continuity shall be enhanced through the use of unique pedestrian hardscape and landscape features, signage and lighting. See following sections for more information.

ii. Facade Design

Facade design guidelines provide criteria to strengthen and promote the civic character and identity of existing and new building facades. In general, these facades should create a sense of democracy and service to the public. Building facades should appear “accessible” and friendly by utilizing materials that create a sense of strength, permanence, while respecting the community’s history and culture. Design guidelines are provided to support the following concepts:

- Respect the community’s culture, traditions and history, while embracing the future by utilizing modern building systems and materials.
- Civic buildings shall appear inviting to the public, creating the sense of accessibility and service to the community.
- Facades shall encourage pedestrian activity along Main Street.

Design Guidelines for storefront level facades for the Main Street Promenade include:

- Where feasible, create a sense of “public access” to main civic building entries by facing main entrances towards Main Street, or align with major civic open spaces or monuments.
- If a building is located on a corner lot, principal facades may be on both street faces, however the main building entry shall face Main Street.



- At existing buildings that do not currently have its main entry facing towards Main Street, provide safe and accessible public paths from main public corridors to building entries. Design elements may include: trellis structures, pergolas, landscaping, signage and or lighting.
- At the ground level and at multi-story atriums or circulation spaces facing Main Street, utilize transparent façade building materials such as storefront glass systems, structural glazing, or glass block. Transparent openings and materials shall cover a minimum of 40 percent of that overall building face.
- Civic buildings shall promote pedestrian activity at the ground level by providing easy to access elements such as public service and information counters, community-use spaces, and even small cafes accessible by the public.
- Entry(s) shall be “grand,” prominent and inviting with high ceilings.
- Entry(s) shall be clearly identifiable and visible from drop off areas, walkways and plazas.

Materials

- To unify both traditional and modern building appearances on new buildings, modern building forms and materials such as exposed steel, concrete and high-performance glass systems are encouraged but shall utilize the scale, rhythm, bulk, setbacks and proportions of existing traditional buildings present along Main Street.
- The use of natural, warm, solid, durable and timeless building materials is suggested. In addition to the materials suggested above, additional building materials may include stone, copper, wood, wrought iron and plaster.
- At building bases, utilize durable building materials such as masonry, stone or concrete

to instill a sense of permanence and strength.

- Finish materials shall be used consistently on all building facades. The design of “stage set” architecture should be avoided.
- Building finishes shall be compatible to each other. The number of finish building materials shall be limited.

Windows & Openings

- Windows shall be placed to create an indoor/outdoor feeling from interior spaces.
- Windows shall be placed to maximize view potential to distant vistas, plazas and garden areas.
- At least 60 percent of the total area of the first floor façade facing Main Street shall be window opening.
- Large openings filled by windows at the pedestrian level create a welcome ambience in a Civic Center District and add interest to the pedestrian experience.
- Building openings shall extend to the structural piers on the sides and lintels above.
- Where feasible, incorporate natural day lighting and natural ventilation features for office and public spaces by utilizing clerestory operable windows, transoms, and atriums. The use of skylights is recommended.

iii. Landscape/Hardscape

Within the Civic Center District, continuing the formal pedestrian friendly promenade by enclosing the street environment with lines of canopy trees is imperative. Low fences, walls or hedges create transitional zones between the public areas of the street and the semi-public spaces such as patios, outdoor vestibules and building courtyards.

Landscape



- Streetscape planting should reflect a consistent alignment of trees with shrubs and groundcover plantings to enhance the visual experience.
- Plantings near buildings and in courtyards, terraces, plazas, and other outdoor spaces for human activity should include trees with large canopies that create shade. Additionally planting schemes that provide visual variety with form, texture and color are encouraged.

Hardscape

- The location of plazas or patios adjacent to public entryways is encouraged to provide an “indoor/outdoor” sense of space and a public gathering space.
- The incorporation of a variety of outdoor spaces including patios, plazas and gardens linked by a pedestrian walkway is encouraged.
- The use of a variety of compatible paving materials at patios, plazas and walkways is encouraged. Special paving patterns and textures shall be used to emphasize important areas or features. Paving materials shall be compatible to materials used on adjacent buildings.

iv. Signage and Identity

The signage and identity guidelines assist with strengthening the pedestrian and wayfinding experience. Civic signage shall also be durable and easy to maintain. Exterior signage for civic facilities shall reinforce the following design concepts:

- Promote the District’s community identity, building distinction and a sense of permanence.
- Identify Main Street as the primary Civic corridor within the Downtown Village.

- Provide informational and directional signage to promote clear and easy access to public buildings and open spaces.
- Encourage and support pedestrian activity along Main Street by providing consistent, easy to read facade signage.

Signage guidelines would include the following:

General Criteria

- Develop an overall signage plan, which utilizes consistent sign types, locations and materials. The plan shall be compatible with other design features and elements.
- Exterior signs shall be located within the base of the building with concern for the appropriateness of location, size, color, and lighting.
- Signs are not allowed on rooftops or projecting above the roof.
- No flashing, or moving signs are permitted.
- Temporary signs in windows are not allowed except under limited conditions for short periods of time.

Building Facade Wall Signs

- Unlike commercial buildings, where signage is a key identity element, signage for Civic Center Buildings shall be modest, timeless and understated.
- Signage materials shall be durable and create a sense of permanence. Recommended materials may include pin-mounted metal cast signage attached to building faces or concrete cast-in-place recessed wall signage.
- The height of letters shall not exceed 18 inches.
- Signage shall compliment the design of building facades.
- Signage colors shall contrast background colors.



Pedestrian Oriented Signage

- Pedestrian-level display window signs applied directly to the glass shall consist only of lettering without an opaque background. On the display window, such a sign shall not cover more than one-fifth of the area of the glass panel.
- Provide directional signage to significant civic buildings and features from prominent pedestrian plazas and corridors as well as from parking lots. See Figure 5.1.

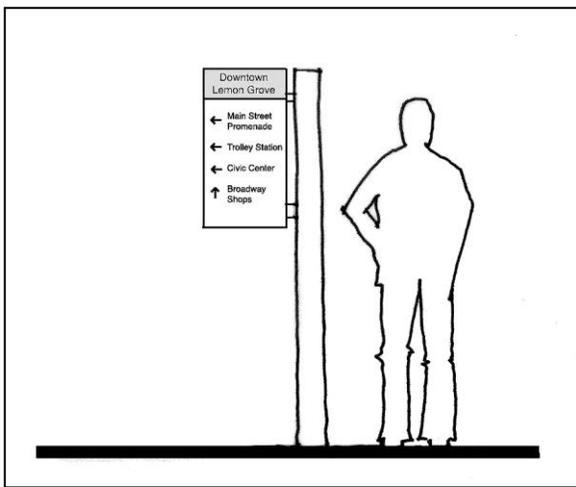


Figure 5.1 Directional Signage

- Provide freestanding, lockable, community information kiosks at prominent pedestrian plazas and corridors. See Figure 5.2.

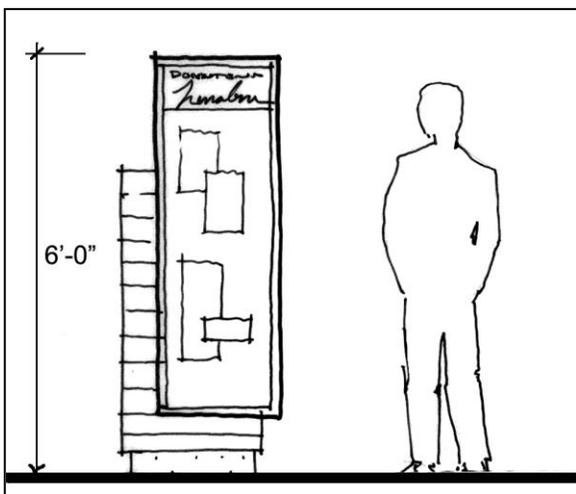


Figure 5.2 Information Kiosk

- Window signs letter colors shall contrast with the display background. Light colored letters or gold leafed letters with dark borders are effective.
- Hanging signs shall project no more than three feet from the face of the building. Brackets and other methods of attachment shall be considered part of the design proposal and shall be designed to be sensitively compatible with the building.

Awnings

- Awnings or canopies shall reflect the door and window openings or structural bays of the building façade.
- Awnings shall be designed in proportion to the building's mass, scale and opening size.
- Awnings can be constructed of several materials including fabric, steel, metal, and wood.
- Fabric awnings shall be made of canvas or Neoprene impregnated fabric. Vinyl awnings are not permitted. The underside of awnings shall be open to expose the structure.

v. Public Spaces

Public space guidelines within the Civic Center District are provided to create comfortable and attractive public "outdoor rooms" by enclosing the street environment with lines of canopy trees and continuous, regular, and visually appealing building fronts.

The public realm includes parks, squares, greens, plazas, playgrounds, trail corridors, and similar urban open spaces. It also includes areas that may not normally be thought of as public spaces, such as parking lots and linkages to them such as paseos or alleys.

- Coordinate the design and placement of high-quality streetscape elements throughout the District.
- Adopt a public art program to strengthen the cultural identity of Lemon Grove. Public art sites shall be located within entries into civic buildings, public parks, pedestrian plazas, landmark locations, and at District entries.
- Install wayfinding and informational signs to increase public understanding of the District.
- All service areas and trash containers must be screened from view through the use of evergreen plant materials, masonry screens or similar solid structures compatible with the building design.
- Landscaping and on-street parking along sidewalks shall provide a sense of spatial enclosure, which typically encourages pedestrian activity.
- Appropriate landscape buffers shall be designed and maintained in parking areas to enhance the external view of the parking area and to screen the parking from the adjacent residences. Landscape buffers can include landscaping, site walls or arcades.

“Paseos”

Where feasible at existing and planned open spaces between buildings that link to parking areas or adjacent neighborhoods, it is recommended to create pedestrian passageways, also known as “Paseos.” Paseos allow property and business owners to increase their façade exposures to the public while providing safe pedestrian-scaled passageways between parking areas to the building frontages. (Figure 4.6) Design criteria includes the following:

- Paseos shall be proportionate to their length and shall be no less than 8’-0” in width.
- Building facades exposed to paseos must have windows or fixed glazing areas of at

least 30 percent of the total surface area of the wall facing the paseo.

- Paseos shall include landscape planters, outdoor public seating spaces, site walls and lighting, and benches within the paseo corridor.
- Paseos may also include shade structures, such as fabric structures or lattice systems to promote a pedestrian scaled environment and provide shade spaces and cover during inclement weather.
- Secondary building entries off of a paseo shall have a projection and/or hanging identity sign that shall project no more than three feet from the face of the building. Brackets and other methods of attachment shall be considered part of the design proposal and shall be designed to be sensitively compatible with the building.

Lighting

- Pedestrian-scaled lighting at the public right-of-ways at the Civic Center District shall compliment the existing lamp fixtures currently installed along Broadway.
- The illumination (warm washing) of Civic Center buildings at night is suggested and will establish prominence and importance.
- Lighting of parking areas and walkways shall exceed minimum standards for these areas and provide a safe and friendly environment, even after dark.
- Lighting fixtures exposed to view in parking areas, walkways and plazas shall be compatible to the architectural design of the Civic Center and shall have a consistent theme.
- All parking lot pole lights shall be limited to a height of sixteen feet and shall be “cut-off” type shrouds to prevent spill over of light onto adjacent properties.



- All pedestrian pole lighting shall be limited to a height of twelve feet.
- No low-pressure sodium lighting shall be permitted.
- Where possible and practical, lighting of walkways, plazas and landscape areas shall be accomplished via indirect lighting (i.e. non-visible fixtures recessed in masonry walls, up lighting of specimen trees, etc.).
- All light fixtures shall be vandal and weather resistant.

Banners

Banners on light poles is an effective way of adding visual interest and drawing attention to special civic events within Downtown Lemon Grove; however, to be effective, banners must be maintained at regular intervals to reflect seasonal changes or announce special civic events.

vi. Parking

Parking and circulation guidelines are provided to enhance public access to civic buildings, plazas and open spaces as well as provide clear and easy circulation, parking and access to principle commercial streets. Parking shall be convenient for customers and visitors and their visual impact shall be softened or minimized. Specific guidelines include:

- Parking lots and access shall be subordinate in design, location and prominence to Civic buildings.
- On-site parking shall be located behind or to the sides of civic buildings in municipal lots, yet shall be conveniently located to minimize walking distance to the Civic buildings.
- Where feasible, consolidate existing parking behind buildings into an adequately sized, convenient, safe and attractive vehicle parking area that will not dominate or

distract from the adjacent uses.

- Surface parking lots shall be screened by landscape features. The screening for parking lots shall include at least four feet (4') of landscaped strip between the vehicle area and the sidewalk with a 30" high screening element of either a dense hedge or a wall or fence integrated with shrub planting.
- All parking lots shall have a curbed perimeter. There shall be a minimum of four feet wide curbed islands at the end of every row of parking and equal to the parking space length. There shall be a minimum of one shade tree per island – two trees per island are required where opposite bays align. Trees within islands shall be aligned between parking spaces.
- Provide clear, accessible and safe pedestrian landscaped pathways from the parking areas to civic building entries, open spaces, and primary streets. Pathways shall have a minimum clear width of 60" and shall meet State and National accessibility requirements. Surface materials shall match new materials proposed in the Main Street Promenade District, and shall include concrete or other durable materials, such as cast-concrete pavers and/or brick.
- All parking areas shall provide adequate lighting at nighttime utilizing fixtures that support the scale, character and identity of the Civic Center District (see lighting standards for specific information).
- Directional signage and/or information kiosks are encouraged within parking lots, key pedestrian corridors and plazas. They shall be visually attractive, complimenting the scale, character and identity of the Civic Center and adjacent Districts (see signage guidelines for specific information).
- The use of pervious paving surfaces is recommended to minimize runoff.



- Parking for the disabled shall be conveniently located adjacent to building entry(s).
- The installation of secured bike storage or bike lockers (for staff) should be considered.
- All parking areas shall comply with the City of Lemon Grove Planned Development Ordinance requirements and with State and Federal accessibility requirements.



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4 - MARKETPLACE

a. Purpose

The Marketplace District is comprised of medium- to large-scale commercial uses. The existing uses are predominantly detached, single-tenant businesses, such as grocery, drugstores or clothing stores with on-site parking. The goal for this District is to encourage enhanced landscaping and signage along the Lemon Grove Avenue frontage and strengthen the pedestrian connection to the Village Core District.

b. Land Use Descriptions

The goal of this District is to provide for the enhancement and continuation of existing commercial uses, as well as the existing civic uses (Figure 3).

i. Commercial: This land use allows retail operations providing a broad range of goods and services, catering to both local and regional customers; including shopping centers, department stores, grocery stores, professional services and other compatible retail businesses.

ii. Civic: This land use allows public uses and service facilities, such as government offices and facilities, schools, public utilities, post office, libraries, and fire and law enforcement stations.

c. Zones, Uses & Development Standards

i. Commercial Zone

This zone allows a variety of medium to large scale commercial sales and services primarily oriented for access by vehicles, with pedestrian access encouraged from the Village Core and the Integrated Transit Station. See Figure 6.

Permitted Uses

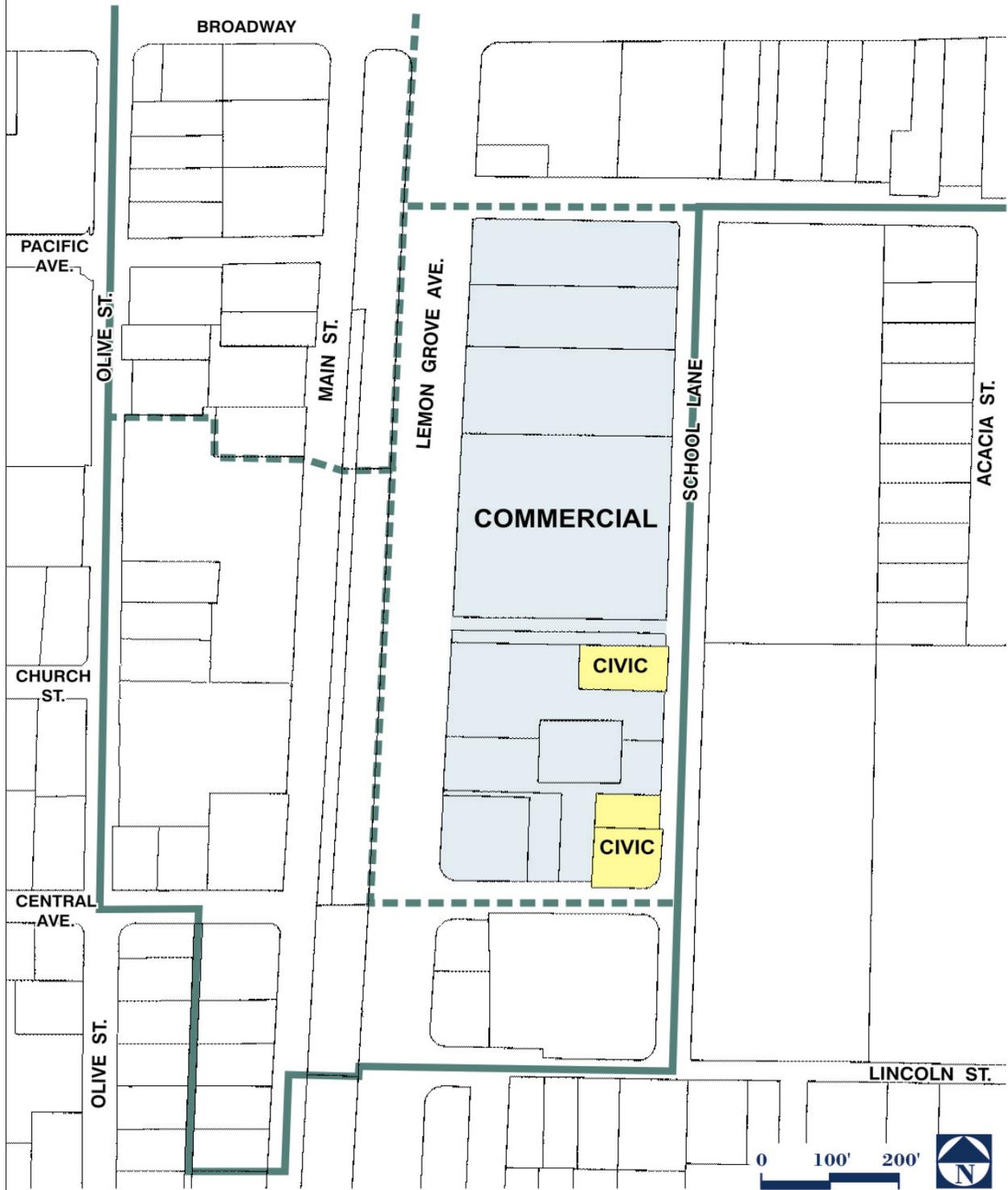
In the General Commercial Zone, the following uses are permitted, subject to the Development Standards in this Section and elsewhere in the Downtown Village Specific Plan.

1. Retail sales activities, generally of a large-scale nature (e.g., supermarkets, specialty goods dealers, dry good stores, department stores, furniture, and appliance dealers, automobile wash establishments); but excluding second-hand merchandise and building supply yards.
2. Retail Service activities of a space intensive nature, offering specialized services of a high quality and including, but not limited to: hair styling, barber and beauty shops, apparel repair, photo processing, locksmith shops, post offices, gymnasium and health studios, pet shops, studios for teaching art, dancing and music but excluding automobile services or equipment rental.
3. Banks, including branch banks and other similar financial institutions.
4. Restaurants and miscellaneous eating places; but excluding drive-in facilities. When such businesses also sell alcohol beverages, a Conditional Use Permit is required.





Marketplace Zones



MARKETPLACE ZONES
Figure 6



Uses Requiring Use Permits

Minor Use Permit

The following uses are permitted subject to the approval of a Minor Use Permit according to the provisions of Section 17.28.052 of the Municipal Code:

1. Business and professional offices.
2. Theaters or playhouses, (no adult theatres shall be permitted) and other forms of entertainment and recreation (e.g., bowling, skating, arcade).
3. Animal hospitals and kennels.
4. Outdoor newsstands attended by salesperson(s); outdoor sale of flowers and similar outdoor activities which are to be conducted on a permanent or regularly recurring basis.
5. Telecommunication and cellular telephone communications facilities.

Conditional Use Permit

The following uses are permitted subject to the approval of a Conditional Use Permit according to the provisions of Section 17.28.050 of the Municipal Code:

1. Public service and utility structures and facilities.
2. Hotels and associated uses.
3. Drive-in or drive-through restaurants, financial institutions and other drive-in or drive-through facilities.
4. Restaurants and miscellaneous eating places when alcoholic beverages are to be sold.
5. Small collection facilities for the recycling of useable materials.
6. Movie theatres.

Development Standards - Commercial Zone

This section establishes site development standards, land use regulations, and other general requirements that apply to all projects in the Commercial Zone. Any standards not specifically covered by this Specific Plan are subject to the regulations of the Municipal Code.

1. Minimum Lot Area

20,000 square feet

2. Minimum lot width and depth

Width – 75 feet

Depth- 120 feet

3. Building Setbacks

- **Lemon Grove Avenue – east side**
Except for the northeast corner of Central Avenue and Lemon Grove Avenue, buildings shall be located no closer than 10 feet from the ultimate right-of-way. Buildings on the northeast corner of Central Avenue and Lemon Grove Avenue shall be located within 5 feet of the ultimate right-of-way.
- **Golden Street – south side**
Buildings shall be located no closer than 10 feet from the ultimate right-of-way.
- **Central Avenue – north side**
Buildings shall be located within 10 feet from ultimate right-of-way.
- **School Lane – west side**
Buildings shall be located no closer than 10 feet from the ultimate right-of-way.

4. Maximum Building Height

45 feet.



5. Mixed Use within same building on same lot

n/a

6. Minimum Usable Open Space

n/a

7. Off-Street Parking

Table D shows the parking calculations for the Marketplace District. This District is calculated to need 1225 parking spaces. This requirement is based on the factors discussed below. The basic parking rate is based on the size of the building and/or the number of dwelling units and includes a reduction for transit. Additional reductions are for the mixed-use nature of the area and for shared parking.

Requirements - The minimum parking rates listed below are specific to the Marketplace District. They reflect reductions from standard suburban rates due to the accessible public transit available in the immediate area. Additional reductions for mixed uses and shared parking are possible.

- a. Office - 5.0 spaces / 1,000 gross square feet
- b. Retail - 5.0 spaces / 1,000 gross square feet

Mixed-Use Reduction- The mixed-use nature of the area and potential redevelopment suggest that a mixed-use parking rate reduction is not warranted.

Shared Parking- This is not applicable to the Marketplace District.

In-lieu fees- This is an opportunity for redevelopment projects to purchase off-site parking cannot reasonably be provided. It is usually applicable in small or narrow lot situations, like the Village Core, where providing on-site parking would remove the building that is generating the parking need and/or the lot is too small for the physical maneuvering of cars. New projects shall be required to provide code required parking on site, but there are situations where it is more efficient for several new projects to provide a single common lot than smaller individual parking lots.

Up to 100 percent of the parking requirement could be satisfied through the payment of an in-lieu fee to be established by the City Council.

On-Street Parking

Credit for on-street parking can be taken from the calculated parking demand for a certain percentage of the parking supply present along a project's lineal frontage. This credit is available for all land use types. The frontage where there are parking meters cannot be included in the frontage available for a parking credit.

| Use | Size | Parking Place | Gross Parking Demand | Mixed-Use Reduction | Shared Parking Reduction @ Noon | Net Parking Demand |
|--------------------|------------|---------------|----------------------|---------------------|---------------------------------|---------------------|
| Market Place | | | | | | |
| -Commercial | 227,209 SF | 5.0/KSF | 1,136 | 1.00 | 1.00 | 1,136 |
| -Civic Office | 17,773 SF | 5.0/KSF | 89 | 1.00 | 1.00 | 89 |
| | | | 1,225 | | | 1,225 |
| Grand Total | | | 1,225 spaces | | | 1,225 spaces |

Notes:
 SF = square feet (gross)
 KSF = 1,000 square feet
 du = dwelling unit



8. Service and Loading

The medium to large scale of these commercial businesses require adequate loading zones. The minimum requirements are:

1 Truck Loading zone for every lot or business up to 100,000 square feet.

1 additional Truck Loading zone for each additional 200,000 square feet.

- No portion of any front yard, or any side yard on the street side of a corner lot, shall be used for off-street loading purposes.
- Fractional loading space requirements shall be counted as a whole space.
- No part of an alley or street shall be used for loading, excepting areas designated by the City for loading in a Minor Use Permit.

9. Street Elements

Sidewalks along the public streets within this zone shall not accommodate outdoor spaces for seating or displays.

10. Landscape

Street trees in public right-of-way. Landscaping in the public right-of-way is intended to create a distinct outdoor character as well as accentuate the strong pedestrian linkage from the Integrated Transit Station to the Civic Center District. The landscaping along the east side of Lemon Grove Avenue provides the opportunity for larger, wide canopy trees that provide shade and a sense of enclosure while still allowing views store identity signage. Street tree selection and any proposed substitutions to the recommended trees shall consider site and maintenance criteria such as planter area, sidewalk location, root damage potential, water and soil requirements, litter, hazards, longevity, and on-street parking location. A limited variety of trees are allowed to strengthen the individual character of the Marketplace District.

- The predominant trees located within the public right-of-way along both sides of Lemon Grove Avenue shall be large, broad canopy trees intended to compliment the formal promenade along Main Street. Trees shall be a minimum size of a 36-inch box and shall be planted on 40' centers. On the east side of Lemon Grove Avenue the street trees shall be planted within the landscaped screening area a minimum of 7 ½ feet from the face of curb. On the west side of Lemon Grove Avenue, the street trees shall be planted a minimum of 7 1/2 feet from the face of curb. These trees shall be limited to:

- Podocarpus gracillior/African Fern Pine
- Tipuana tipu/Tipu Tree
- Pinus pinea/Italian Stone Pine
- Koelreuteria bipinnata/Chinese Lantern

- Trees located within the right-of-way along Central Avenue and School Lane, shall have a more vertical characteristic to soften the large buildings within the Marketplace District from adjacent development. Trees shall be a minimum size of a 24-inch box and shall be planted at the rate of one for every 25 feet of frontage located behind curb in tree wells a minimum of 3'x 3'. Tree wells shall be covered with tree grates approved by the City Council. These trees shall include:

- Bauhinea purpurea/Purple Orchid Tree
- Bauhinea v. candida/White Orchid Tree
- Hymenosporum flavum/Sweet Shade
- Podocarpus macrophyllus, Yew Pine
- Pinus canariensis, Canary Island Pine
- Tristania conferta/Brisbane Box

Other similar trees may be allowed with the approval of the Director.

Landscaping on private property. In any property development, an area creating comfortable and attractive outdoor areas shall include landscaping.

- All landscaping shall be shown on a detailed landscape plan approved by the Director.



- All planting areas shall be served by permanent irrigation systems.

11. Enclosures/Screening

- The screening for parking lots shall include at least four feet (4') of landscaped strip between the vehicle area and the sidewalk with a 30" high screening element of either a dense hedge or a wall or fence integrated with shrub planting.

12. Signs

- Signs in the transom area shall not exceed 65 percent of the width of the panel or 75 percent of the height of the panel.
- The height of letters shall not exceed 18 inches.
- Projecting signs shall be mounted on the structural piers of the building. They shall be no lower than 8'6" and no higher than the base of the building, or 14 feet, whichever is lower.

13. Nonconforming Use/Lot

Pursuant to Section 17.24.090 of the Municipal Code.

14. Temporary Use

Pursuant to Section 17.28.040 of the Municipal Code.

ii. Civic Zone

This zone is intended to provide an area for uses and services typically provided to residents by the City. This includes uses and services such as government offices, public utilities, post office, libraries, and fire and law enforcement stations (Figure 6).

Permitted Uses

In the Civic Zone, the following uses are permitted, subject to the Development Standards in this Section and elsewhere in the Downtown Village Specific Plan.

1. Government Offices and Facilities.
2. Cultural Activities (e.g., library, museum, gallery).
3. Fire and Law Enforcement Stations.
4. Social Service Facilities.
5. Community Centers.
6. Parks.
7. Any other business or service establishment which the Planning Director finds to be consistent with the purposes of this Specific Plan and the intent of the General Plan.

Uses Requiring Discretionary Use Permits

Minor Use Permit

The following uses are permitted subject to the approval of a Minor Use Permit according to the provisions of Section 17.28.052 of the Municipal Code:

1. Telecommunication and cellular telephone communications facilities.

Conditional Use Permit

The following uses are permitted subject to the approval of a Conditional Use Permit according to the provisions of Section 17.28.050 of the Municipal Code:

None.

Development Standards – Civic Zone

This section establishes site development standards, land use regulations, and other general requirements that apply to all projects in the three Transit Mixed Use Zones. Any standards not specifically covered by this Specific Plan are subject to the regulations of the Municipal Code.



1. Minimum Lot Area

No minimum lot area requirement.

2. Minimum lot width and depth

No minimum lot width or depth requirement.

3. Building Setbacks

School Lane-west side

Buildings shall be located no closer than 10 feet from the ultimate right-of-way.

Central Avenue- north side

Buildings shall be located no closer than 10 feet from the ultimate right-of-way.

4. Maximum Building Height

45 feet.

5. Mixed Use within same building on same lot

n/a

6. Minimum Usable Open Space

n/a

7. Off-Street Parking

See Parking Standards set forth in the Commercial Zone. Table D includes standards for the entire District, including Medium Residential.

8. Service and Loading

The variety of public uses and services provided in this zone requires a variety of service areas and loading spaces. The individual service and loading requirements for each civic use shall be considered and specifically reviewed prior to the approval of any project within this District to guarantee that adequate service areas and loading zones are provided.

- No portion of any front yard, or any side yard on the street side of a corner lot, shall be used for off-street loading purposes.

- Fractional loading space requirements shall be counted as a whole space.
- No part of an alley or street shall be used for loading, excepting areas designated by the City for loading in a Minor Use Permit.
- Requirements for uses not specifically listed herein shall be based upon the requirements for comparable uses listed and upon the particular characteristics of the use as determined by the Planning Director.

9. Street Elements

Sidewalks along the public streets within this zone shall not accommodate outdoor spaces for seating or displays.

10. Landscape

Street trees in public right-of-way. Landscaping in the public right-of-way is intended to create a distinct outdoor character. Street tree selection and any proposed substitutions to the recommended trees shall consider site and maintenance criteria such as planter area, sidewalk location, root damage potential, water and soil requirements, litter, hazards, longevity, and on-street parking location. A limited variety of trees are allowed to strengthen the individual character of the Market Place District.

- Trees located within the right-of-way along Central Avenue and School Lane, shall have a more vertical characteristic to soften the large buildings within the Marketplace District from adjacent development. Trees shall be a minimum size of a 24-inch box and shall be planted at the rate of one for every 25 feet of frontage located behind curb in tree wells a minimum of 2'x 2'. Tree wells shall be covered with tree grates approved by the City Council. These trees shall include:
 - Bauhinea purpurea/Purple Orchid Tree
 - Bauhinea v. candida/White Orchid Tree
 - Hymenosporum flavum/Sweet Shade
 - Podocarpus macrophyllus, Yew Pine
 - Pinus canariensis, Canary Island Pine
 - Tristania conferta/Brisbane Box



Other similar trees may be allowed with the approval of the Director.

Landscaping on private property. In any property development, an area creating comfortable and attractive outdoor areas shall include landscaping.

- All landscaping shall be shown on a detailed landscape plan approved by the Director.
- All planting areas shall be served by permanent irrigation systems.

11. Enclosures/Screening

- The screening for parking lots shall include at least four feet (4') of landscaped strip between the vehicle area and the sidewalk with a 30" high screening element of either a dense hedge or a wall or fence integrated with shrub planting.

12. Signs

- Exterior signs shall be located on the ground level of the building or on a prominent monument signage located near major public circulation corridors such as Main Street.
- The height of the letters shall not exceed 18 inches.
- Signs are not allowed on rooftops or projecting above the roof, eaves, or parapets.
- Signage may not exceed 50 square feet in area and may not exceed eight (8) feet in height.
- No flashing, or moving signs are permitted.

13. Nonconforming Use/Lot

Pursuant to Section 17.24.090 of the Municipal Code.

14. Temporary Use

Pursuant to Section 17.28.040 of the Municipal Code.

While Broadway is considered the central business corridor through Lemon Grove, the Market Place District compliments Downtown Lemon Grove by offering additional community-oriented businesses and services not available within the Village Core or Main Street Promenade Districts.

As described earlier, due to the geographic location of the District east of Lemon Grove Avenue, the Marketplace District is comprised of medium to large-scale commercial uses. The uses are currently detached, single-tenant businesses, such as grocery, drugstores or clothing stores that accommodate direct vehicular access and on-site parking. The intent of the design guidelines for this District is not to change the type of use.

d. Design Guidelines – Market Place District

i. Site Planning and Street Design

Site Planning and design criteria are provided to support the goals of creating a pedestrian-friendly walking environment along Lemon Grove Avenue and to strengthen the pedestrian connections within the District and to adjacent Districts. Design criteria are also presented to strengthen the southern gateway into Downtown area through development design standards.

Design Criteria include:

- Provide a pedestrian-friendly walking corridor along Lemon Grove Avenue by providing a landscaped buffer between the parking lot and public sidewalk and between the street and sidewalk utilizing street trees See Figure 6.1.



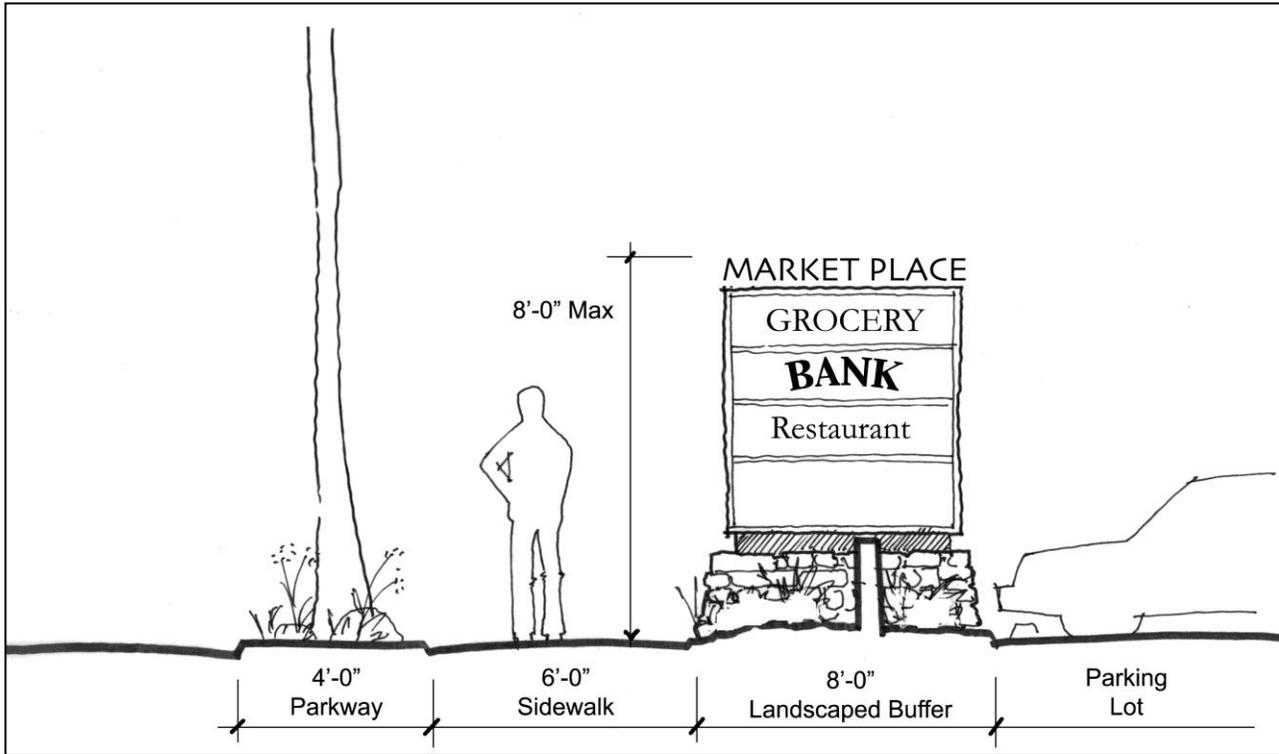


Figure 6.1 Marketplace Landscape Buffer

- On the larger parcels, it is recommended to develop a pedestrian corridor that links Lemon Grove Avenue to School Lane. The corridor shall include elements that define a pedestrian-oriented streetscape such as shade trees, landscaped parkways, sidewalks and enhanced paving.
- New service alleys shall be designed with minimum intrusion and visibility from public spaces. Existing service access and alleys shall be visually screened from public view utilizing landscaping site walls, pergolas or trellises.
- To encourage the future development of a southern “gateway”, new buildings shall define a strong corner at the Northeast corner of Central Avenue and Lemon Grove Avenue. Building frontages shall be set close to the street edge with a 5’ setback from the property line along Lemon Grove Avenue and 10’ on Central Avenue. See Figure 6.2.

ii. Storefront Design

Design standards for storefronts provide criteria that promote quality design for entry facades of new buildings and rehabilitation of existing buildings. The criteria presented below are intended to supplement existing Downtown Storefront Design Guidelines provided under a separate document. Additional guidelines include:

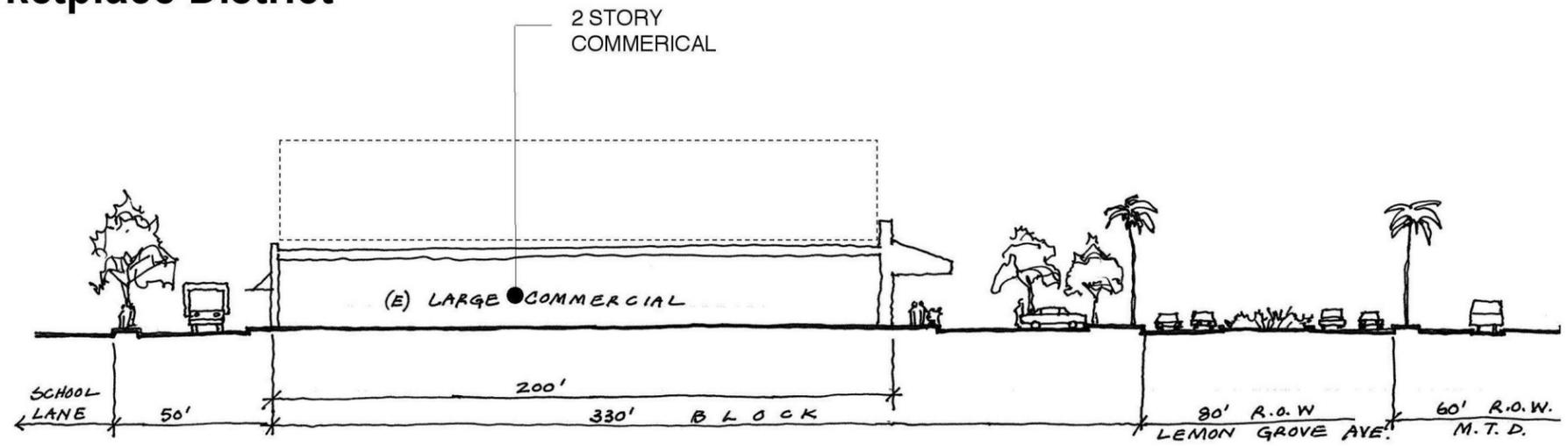
- The storefront is defined as that part of the building that fills the structural bay on the front façade at ground level. For new buildings, primary storefronts shall face the principal street – i.e. Lemon Grove Avenue. If a building is located on a corner lot, storefronts may be on both street faces, yet the primary storefront facade shall face the principal street.
- Storefront openings shall be proportionate to the building and at least 30 percent of the total area of the west facades facing Lemon



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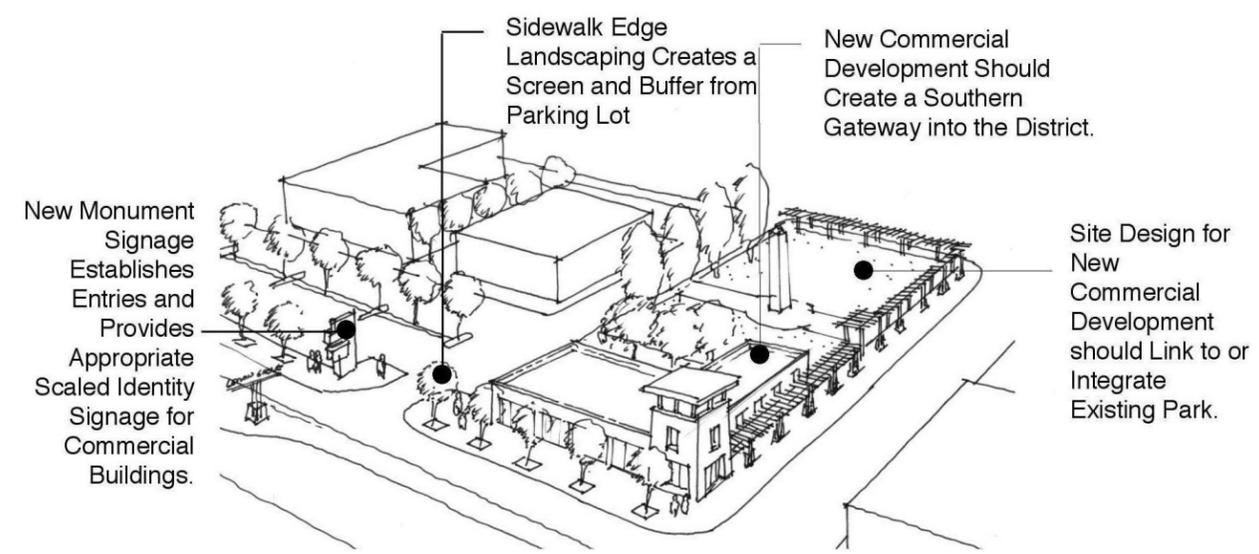


Marketplace District



Section G - Lemon Grove Ave.

No Scale



Bird's Eye View of Northeast Corner of Lemon Grove Ave. and Central Ave.



City of Lemon Grove Downtown Village Specific Plan



Roesling Nakamura Architects, Inc.

Figure 6.2

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Grove Avenue and at the front elevation (if the two facades are not the same) of the building shall be glazed.

- Walls exposed to side streets (such as Golden Avenue and Central Avenue) or pedestrian paseos must have doors and/or windows, or fixed glazing areas of at least 25 percent of the total surface area of the wall facing these open spaces.
- Storefront materials and colors shall compliment the existing architectural character of buildings within this District. Materials for storefront structural mullions shall include aluminum, steel, wood, and glass.
- Interior security gates or enclosures shall be retractable during business hours. No exterior security gates or bars are permitted.
- Avoid flat facades by using recessed or projected entryways, bays, canopies, awnings and other architectural elements.

iii. Landscape/Hardscape

Landscape and hardscape design criteria goals are to enhance the landscaping and signage along the Lemon Grove Avenue and strengthen the pedestrian connection with the Village Core District.

Landscape

- Streetscape planting should reflect a consistent alignment of trees with shrubs and groundcover plantings to enhance the visual experience.
- Plantings near buildings and in courtyards, terraces, plazas, and other outdoor spaces for human activity should include trees with large canopies that create shade. Additionally planting schemes that provide visual variety with form, texture and color are encouraged.

Hardscape

- At the intersection of Lemon Grove Avenue and Central Avenue, crosswalks shall utilize enhanced paving materials such as stamped concrete, brick pavers, or enhanced asphalt surfacing to emphasize this intersection as the southern gateway into Downtown Lemon Grove. Enhancements shall match those in neighboring Districts.
- Mid-Block tree-lined pedestrian passage ways that link Lemon Grove Avenue and School Lane shall have enhanced paving materials that match those found at crosswalks described above.
- Enhanced paved crosswalks shall be included that link the northern boundary of the District with the southern boundary of the Village Core District. These crosswalks shall utilize the same materials described above.

iv. Signage and Identity

The Signage and Identity criteria are intended to unify the graphic character of signs advertising a business within the Market Place District.

While the “Main Street” theme encourages traditional storefront signage with the intent to communicate to the pedestrian, the theme for this District promotes graphic communication of businesses at two levels. These include:

1. Entry Monument Signage
2. Building Signage

In addition to these standards, commercial signs are regulated by the City of Lemon Grove Planned Development Ordinance requirements.

Entry Monument Signs

Entry monument signs are freestanding signs set apart from the building structure to signify entries into major commercial establishments. Unlike other Districts within the Downtown Village, entry monument signs along Lemon



Grove Avenue are encouraged. These signs help define the District's character, strengthen the street's urban edge, identify major driveway entries, and unify the various District signs. The guidelines below are governed by the City of Lemon Grove Sign Ordinance for freestanding signs.

- Each major establishment or shopping center may qualify for on freestanding sign located near each driveway entrance fronting an arterial street.
- Signage shall be clearly visible both day and night from passing cars along Lemon Grove Avenue and shall also identify primary driveways onto the block from Lemon Grove Avenue.
- To qualify for this type of signage, the property parcel must have at least 150 feet of continuous street frontage facing Lemon Grove Avenue.
- Sharing of signage between tenants is encouraged where feasible.
- Signage structure design shall compliment a proposed Downtown monument sign to be located on the center median at Broadway by utilizing similar materials and construction.
- Signs may be externally or internally lit.
- Monument signs shall be accentuated with landscaping to distinguish it from secondary points of entry.
- Signs shall not encroach into the public right-of-way at both the base and /or with overhead structures.

Exterior Building Signs

Building facade signage shall be able to clearly communicate the identity of the business from traffic along Lemon Grove Avenue and shall visually relate to the scale and massing of larger buildings.

The upper portion of the building is the most visible part of the building both to pedestrians and to motorists. Signs also contribute to the overall image of a district, adding variety and liveliness to both building facades and to streetscapes.

General Criteria

- Exterior building signs shall not extend more than three (3) feet in height above the ridge line of the roof or parapet wall.
- Signs are typically intended to be viewed by motorists and pedestrians and shall be oriented to establish the location, identity, and character of the use of the retail establishment.
- Businesses shall only include the formal name of the business, the nature of the business and the address. There shall be no advertising of brand names.
- Temporary advertising signs, sale signs, lease signs, etc. attached to the building and display windows shall be limited to 30 days of display time.
- To encourage diversity in design, there are no specific fonts or letter style that is recommended; however, a letter style shall be chosen that is easy to read and that represents the image of the business it is presenting.
- Temporary signs in windows are not allowed except under limited conditions for short periods of time.

Building Facade Wall Signs

- The location of the sign shall be placed on the "qualifying" wall (wall facing principal street) and shall not exceed 20 percent of the building face, up to a maximum of 250 square feet.



Pedestrian Oriented Signage

- Pedestrian-level display window signs applied directly to the glass shall consist only of lettering without an opaque background. On the display window, such a sign shall not cover more than 40 percent of the area of the glass panel.
- Window signs shall not obscure the display area. The color of letters shall contrast with the display background. Light colored letters or gold leafed letters with dark borders are effective.
- Hanging signs shall project no more than five feet from the face of the building. Brackets and other methods of attachment shall be considered part of the design proposal and shall be designed to be compatible with the building.

Projecting Signs

- Projecting signs shall be mounted on the structural piers of the building. They shall be no lower than 8'6" and no higher than the top of the building or fifteen (15) feet, whichever is lower.
- Projection signs shall compliment the architecture of a building in respect to colors, materials, and structure. Only one projection sign shall be allowed per building.
- All projecting signs can be externally or internally lit. External light sources shall be shielded from viewers on the street to prevent glare.

Awnings

- Awnings or canopies shall reflect the door and window openings or structural bays of the building façade.
- Awnings shall be proportionate to the building size.

- Awnings can be constructed of several materials including fabric, steel, metal, and wood.
- Fabric awnings shall be made of canvas or neoprene impregnated fabric. Vinyl awnings are not permitted. The underside of awnings shall be open to expose the structure. Bright and neon colors are not permitted.
- Signs on awning shall have contrasting letters painted or sewn onto the valance or skirt of the awning. Six to eight inch letters are recommended.
- Letters on the main, sloping area of the awning are not permitted.

v. Public Spaces

Within the Market Place District, public spaces shall include the sidewalks along the District perimeter, pedestrian corridors linking Lemon Grove Avenue and School Lane, and the open space park at the northwest corner of Central Avenue and School Lane.

vi. Streetscape Elements

- Streetscape elements shall be the unifying feature of all four Downtown Districts. Streetscape elements shall compliment the traditional "Main Street" theme and items shall include; tree grates, benches, directional signage, street name signage, freestanding information kiosks, trash cans, lighting fixtures, bus stops shelters and benches.
- Adopt a public art program to strengthen the cultural and/or historical identity of Lemon Grove. Within the Market Place District, Public art sites may include the following location: Civic Park at the intersection of Central Avenue and School Lane.
- Install pedestrian-scaled wayfinding/ direction and informational signs to increase public understanding of the District.



- All service areas and trash containers shall be screened from view through the use of dense evergreen plant materials and decorative masonry screens or similar solid structures compatible with the building streetscape design.
- Appropriate landscape buffers shall be designed and maintained in parking areas to enhance the external view of parking area and to screen the parking from the adjacent residences. Landscape buffers can include landscaping, or landscape elements.

Pedestrian “Paseos”

Where feasible at existing and planned open spaces between buildings that link to parking areas or adjacent neighborhoods, create pedestrian passageways also known as “Paseos.” Paseos allow property and business owners to increase their façade exposures to the public while providing safe pedestrian-scaled passageways between parking areas to the building frontages. Design criteria include the following:

- Paseos shall be no less than 8’-0” in width.
- Building facades facing paseos shall have windows or fixed glazing areas of at least 30% of the total surface area of the wall facing the paseo.
- Paseos shall include landscape planters, outdoor public sitting spaces, site walls, lighting, and benches within the paseo corridor.

Paseos may also include shade structures such as fabric structures or lattice systems to promote a pedestrian scaled environment and provide shade spaces and cover during inclement weather.

Lighting

Pedestrian-scaled lighting at the public right-of-way and at commercial property shall compliment the existing lamp fixtures along

Broadway.

Banners

Banners on light poles as an effective way of adding visual interest and drawing attention to special events within Downtown Lemon Grove. However, to be effective, banners must be maintained at regular intervals to reflect seasonal changes or announce special events.

vii. Parking

Parking and circulation guidelines are provided to enhance the identity of the Market Place District. Signage guidelines are provided that will direct both vehicles and pedestrians to parking and other destinations within the downtown area. Design criteria include:

- Access to parking shall be from Lemon Grove Avenue or from School Lane.
- Where feasible, consolidate existing smaller parking lots into adequately sized and convenient, safe and attractive vehicle parking area that will not dominate or distract from the adjacent uses.
- There shall be a minimum of four feet wide curbed islands at the end of every row of parking and equal to the parking space length. There shall be a minimum of one shade tree per island – two trees per island are required where opposite bays align. Trees within islands shall be aligned between parking spaces.
- Provide clear, accessible and safe pedestrian landscaped pathways from the parking areas to principle commercial streets. Pathways shall have a minimum clear width of 60” and shall meet State and National accessibility requirements. Surface materials shall include concrete or other durable materials, such as cast-concrete pavers and/or brick.
- Provide landscape buffers or screening elements that separate public right-of-ways



from parking lots and to help soften the visual impact of parking lots edge (Figure 6.1).

- Buffers or screens along Lemon Grove Avenue shall be composed of a minimum four feet (4') of landscaped strip between the vehicle area and the public sidewalk with a 42" high screening element of either a dense hedge, a wall or fence integrated with shrub planting. Landscape buffers may also include site walls or arcades.
- All parking areas shall provide adequate lighting at nighttime utilizing fixtures that support the scale, character and identity of the Market Place District (see lighting standards for specific information).
- Directional signage and/or information kiosks within parking lots are encouraged and shall be visually attractive, complimenting the scale, character and identity of the Market Place District (see signage guidelines for specific information).
- All parking areas shall comply with the City of Lemon Grove Planned Development Ordinance requirements and with State and Federal accessibility requirements.



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CHAPTER IV – MOBILITY, PUBLIC SIGNAGE & PARKING

This chapter includes information on vehicular circulation, transit, bikeways, pedestrian walkways, public signage and parking. The information is divided into existing conditions and proposed design standards. The goals of this element of the Specific Plan is to identify standards for mobility, public signing, and parking to be coordinated with high density development within the new planning districts.



A. Vehicular Circulation

A.1 Existing Conditions

The Downtown Village Specific Plan includes a number of circulation element roads. The roads and arterials are one important segment of automobile and pedestrian mobility within the Specific Plan.

The following is a brief summary of the two major roadways in the project area:

Broadway is classified as a four-lane Major Road on the Lemon Grove Circulation Element. In the project area, Broadway runs east-west, parallel with SR 94. Curbside parking along Broadway is generally prohibited due to the presence of Class III bike lanes; however in the vicinity of the Village Core, parking along Broadway is prohibited. Parking for local businesses along Broadway between Lemon Grove Avenue and Grove Street is served by diagonal off-street parking areas on both sides of Broadway. The San Diego Trolley Orange Line

crosses Broadway at-grade just west of Lemon Grove Avenue and Lemon Grove Avenue at-grade just west of Lemon Grove Way.

Lemon Grove Avenue is classified as a four-lane Major Road in the Lemon Grove Circulation Element. Lemon Grove Avenue runs north-south from its northern terminus at SR 94, just north of the project area, to the southern boundary of the City. Within the project area, curbside parking is generally prohibited, with the exception of some sections of parallel parking northbound near Broadway. The San Diego Trolley Orange line is parallel and to the west of Lemon Grove Avenue. At-grade crossings of the Trolley line are reduced to minimize Trolley conflicts with vehicular traffic.

The Downtown Village Specific Plan anticipates three important future projects; the realignment of Lemon Grove Avenue, the Main Street Pedestrian Promenade with an Integrated Transit Plaza and Station and the grade separation of the San Diego Trolley at both Broadway and Lemon Grove Avenue.

The realignment of Lemon Grove Avenue, although not within the Downtown Specific Plan, was a General Plan Amendment adopted by the City Council in June 2007 and included a general discussion of the Main Street Promenade. The realignment of Lemon Grove Avenue considered the closure of Main Street to enhance the circulation patterns at the main entry of the city, to reduce traffic congestion in the downtown area and to promote a pedestrian friendly environment along the Main Street Promenade. The construction of the realignment project is expected to commence in late 2009.

The Main Street Pedestrian Promenade is a concept that eliminates all vehicular traffic circulation on Main Street between Broadway and North Avenue and eliminates through traffic from Pacific Avenue onto Broadway via Main Street. The Main Street Promenade will cater to the needs of the pedestrian while providing strategic and unobtrusive places for vehicle parking and circulation. The promenade concept includes the relocation of the existing pedestrian crossing on Broadway from the east of the trolley/railroad tracks to the west side of the

trolley tracks. The promenade also includes the relocation of the existing bus stop currently located south of Broadway on Main Street to a stop on the south side of Broadway. The new bus stop location and pedestrian crossing provide a more direct, convenient and timely transfer of transit passengers coming and going between the buses, trolley, businesses and residential units. The promenade concept creates a high quality pedestrian realm that will include opportunities for public art, outdoor dining, passive recreation, landscape enhancements, lighting, signage, seating, and safe, convenient access to the bus and trolley.

The Main Street Promenade **north** of Broadway incorporates a public linear park, a transit plaza next to the existing trolley station, and a dedicated lane for the bus stop located on the north side of Broadway. Future private development is expected to incorporate a transit “kiss and ride” drop-off area for trolley riders. Main Street from North Avenue to Broadway will be vacated as a public street and allows for the integrated design of an expanded future development footprint connecting to a strong public pedestrian edge and convenient access to transit services.

The Main Street Promenade **south** of Broadway includes an expansion of the pedestrian plaza around the existing Lemon Monument and eliminates through-traffic between Main Street and Broadway. The new expanded plaza serves as an area for pedestrians waiting to cross Broadway and an important enhanced pedestrian path. The bus stop is relocated from Main Street to a dedicated lane on the south side of Broadway and the area currently utilized by the bus stop on Main Street is converted to vehicle parking area and an enhanced pedestrian plaza.

The Main Street Promenade will occur in phases as development on adjacent properties is constructed. The interim phases are shown in Figure 7. Each phase will encompass safe, convenient, attractive, accessible and comfortable public gathering and walking spaces. The ultimate design concept for the Main Street Promenade is shown on Figure 8.

The grade separation project will significantly reduce the barriers to pedestrian circulation across Broadway and Lemon Grove Avenue as well as ease conflicts with vehicular traffic. The immediate termination of traffic signal preemption by the Trolley would significantly reduce traffic congestion, increase pedestrian safety and allow direct pedestrian crossing of Broadway at Main Street.

A.2 Proposed Street Standards

The streets in Downtown Lemon Grove provide for vehicular circulation throughout the area and access to parking spaces. The improvement standards for streets as identified within the Specific Plan are as follows:

Broadway

- Four travel lanes – two lanes in each direction.
- Center landscaped median.
- Diagonal parking separated from travel lanes by parking median.

Lemon Grove Avenue

- Four travel lanes – two lanes in each direction.
- Center landscaped median.
- Parallel parking generally prohibited, only allow on east side when a 20-foot wide curb lane is available.

Lester Avenue

- Two travel lanes – one lane in each direction.
- Parallel parking allowed.

Golden Avenue

- Two travel lanes – one lane in each direction.
- Parallel parking allowed.

Grove Street (west side from Broadway to Specific Plan Boundary)



- Two travel lanes – one lane in each direction.
- Encourage extension of vehicular circulation from southerly terminus of Grove Street to Golden Avenue. This may take the form of a street, alley or parking area.
- Parallel parking allowed.

Kempf Avenue (south of Broadway to Golden Avenue)

- Two travel lanes – one lane in each direction.
- Parallel parking allowed.

Main Street Promenade (from Broadway to North Avenue)

- Development footprints (requires public road vacation process).
- Pedestrian pathways and plazas.
- Landscape enhancements.
- Access for Fire and Emergency and Maintenance vehicles.

Main Street (east and west sides from Broadway to Pacific Avenue)

- Pedestrian Plaza along Broadway. No through traffic to Broadway.
- Two travel lanes – one lane in each direction south of the pedestrian plaza.
- Allow parking on both sides south of pedestrian plaza.

Main Street (east and west sides from Pacific Avenue to Central Avenue)

- Two travel lanes – one in each direction.
- Parallel and perpendicular parking allowed.

North Avenue (south side from Main Street to Olive Street)

- Four travel lanes – two lanes in each direction with turn lanes.
- No on-street-parking allowed.

Pacific Street (north and south sides from Main Street to Olive Street)

- Two travel lanes.
- Parallel parking allowed.

Olive Street (east side from Civic Center District to North Avenue)

- Four travel lanes – two lanes in each direction.
- Turn lanes.
- Parallel parking allowed.

Central Avenue

- Two travel lanes – one lane in each direction.
- Parallel parking allowed.

School Lane

- Two travel lanes – one lane in each direction.
- Parallel parking allowed.

B. Transit

B.1 Existing Conditions

The Downtown Specific Plan Area has a comprehensive mix of public transit services, consisting of the San Diego Trolley and local bus routes. The current circulation patterns for the transit services and pre-emption of traffic signals by the Trolley contribute to congestion in the downtown area. Currently, the local bus stops situated south of Broadway on Main Street, away from the existing Trolley station, creates both pedestrian and vehicle congestion at the rail crossing at both Broadway and Lemon Grove Avenue.

B.2 Proposed Transit Standards and Phasing

The current pre-emption of traffic signals by the Trolley at the two critical intersections of Broadway/Lemon Grove Avenue and Lemon Grove Avenue/Lemon Grove Way creates significant traffic congestion, traffic progression,

and pedestrian safety problems. The preemption of the traffic signals at these intersections sever the critical proposed pedestrian and vehicular linkages within the Village Core that are necessary for the success of the transit-oriented development and higher residential densities proposed by this Downtown Village Specific Plan, and the long-term viability of Lemon Grove's Smart Growth planning efforts. Left unresolved, these traffic congestion and pedestrian safety problems will severely limit the ability of the City to implement the Downtown Village Specific Plan. In order to minimize these adverse impacts resulting from the operation of the Trolley in Lemon Grove, and until the grade separation of the Trolley line can be achieved, it is imperative that the Trolley be controlled by traffic signalization at both of these crucial intersections. The Trolley can no longer be the highest priority traffic at these intersections and the current traffic signal pre-emption by the Trolley and the resulting congestion and traffic and pedestrian safety problems must be eliminated for this Specific Plan to be successful and for the future viability of downtown Lemon Grove.

- The bus stops and Trolley station should be integrated to allow for a multi-modal opportunity. The emphasis will be on providing multi-modal opportunities to higher density residential within walking distance to an integrated Trolley station and bus stop. The initial phasing of the Downtown Village Specific Plan requires a local bus stop to be relocated to the proposed site. To make pedestrian access between the eastbound bus passengers and Trolley passengers the most convenient, a primary decorative pedestrian crossing on Broadway would be to the west side of the Trolley tracks.
- A critical long-term solution for the enhancement of the Downtown Village Specific Plan automobile, pedestrian, and transit mobility is the grade separation of the San Diego Trolley. The plan requires a long-term goal for the City of Lemon Grove to work with MTS to lower the Trolley along Lemon Grove Avenue to travel beneath

Broadway and the portion of the future North Avenue that provides access to Highway 94.

C. Pedestrian/Bicycle Circulation

C.1 Existing Conditions

The following is a discussion of the existing pedestrian circulation in the project area, particularly at the Broadway / Lemon Grove Avenue intersection.

The existing Lemon Grove downtown village center has potential for substantial pedestrian circulation. SANDAG records show that Lemon Grove Trolley station is the 3rd most active stop on the Orange Line in terms of passenger on-off activity. This indicates that pedestrians are embarking and disembarking in relatively large numbers at this location. In addition to the Trolley, two bus lines (856/936) have stops within 2 blocks of the Trolley station.

Broadway east of Lemon Grove Avenue has a high concentration of local businesses including restaurants, merchandisers and services, while Lemon Grove Avenue south of Broadway has several national chain stores. All of these factors combine to create a high level of commuter/consumer services that encourage pedestrian activity.

The largest obstacle to both pedestrian and traffic circulation in the downtown village is the at-grade crossing of the San Diego Trolley Orange Line with Broadway. Impediments to pedestrian circulation include restricted access north-south across Broadway at Main Street west of the tracks, and blocking of east-west traffic across Lemon Grove Avenue when a Trolley arrives due to signal operations being preempted by the Trolley.

Pedestrians are prohibited from crossing north-south across Broadway at Main Street because of geometric considerations with vehicular traffic. To cross on the west side of Lemon Grove Avenue from the Trolley Station north of Broadway to bus stops south of Broadway, the pedestrian must cross east across the Trolley



tracks, south across Broadway, then west back across the tracks. This inconvenient circulation pattern is compounded by the high traffic volumes on both Lemon Grove Avenue and Broadway, which result in long waits for pedestrian crossings. As a result, many pedestrians simply jaywalk from one side of Broadway across to the other. An alternative solution to eliminate the temptation for bus passengers to jaywalk cross Broadway at Lemon Grove Avenue and Main Street would be the relocation of the bus stop on Main Street south of Broadway to the southeast corner of Broadway and Lemon Grove Avenue (Interim Integrated Transit Station Circulation). The future grade separation of the Trolley from Broadway will resolve this problem.

A second undesirable situation for pedestrians is the preemption of east-west traffic on Broadway by the Trolley. When a Trolley is approaching, the signal on Lemon Grove Avenue quickly turns red, while eastbound traffic on Broadway receives the green light so that the tracks can be cleared. If a pedestrian is crossing Broadway at Lemon Grove Avenue when a Trolley arrives, the signal will quickly change from green to red, potentially stranding pedestrians in the crosswalk. While this may not occur regularly, it is an unsafe situation, and unavoidable with the current at-grade crossing. The elimination of traffic signal preemption by the Trolley would significantly reduce traffic congestion, increase pedestrian safety and allow direct pedestrian crossing of Broadway at Main Street. The likelihood of achieving elimination of the trolley preemption has required that the City consider other solutions and the relocation of the pedestrian path to the west side of the tracks appears to be the only immediately feasible solution. The future grade separation of the Trolley from Broadway will also resolve this problem.

Located both south and east of the Lemon Grove Avenue/Broadway intersection are the national retailers and other local businesses. Sidewalks are provided on east side (northbound) of Lemon Grove Avenue near the national retailers. The pedestrian is separated from the retailers by large parking areas, which prevents the

provision of a functional and pleasant pedestrian experience.

The local retailers along both sides of Broadway have pedestrian thoroughfares directly along the frontage of their businesses. While the long, uninterrupted pedestrian thoroughfares along both sides of Broadway have desirable characteristics, the city found that there were only two safe locations to cross Broadway at a protected (signalized) location: Lemon Grove Avenue and Grove Street. The city recently installed a protected mid-block crosswalk between these two streets greatly enhancing pedestrian circulation in this district.

The City of Lemon Grove has implemented the General Plan by designating a Class II Bicycle Lane on Lemon Grove Avenue and a Class II Bicycle Lane and Class III Bicycle Route on Broadway within the Downtown Village Specific Plan.

C.2 Proposed Pedestrian/Bicycle Circulation Design Standards

Streets in the Downtown Village provide for pedestrian circulation throughout the area and will provide access from the transit station and parking spaces to businesses and other downtown destinations. The improvement standards for pedestrian circulation are as follows.

Broadway

- Sidewalks adjacent to right-of-way on both north and south sides. Allow outdoor uses of public sidewalk that provide for a minimum of 5 feet of unobstructed sidewalk width.
- Enhanced pedestrian crosswalks at Lemon Grove Avenue, Olive Street, and Grove Street intersections.

Lemon Grove Avenue

(east and west sides from Lincoln Street to Golden Avenue)

- Sidewalks adjacent to right-of-way on east side.

- Enhanced pedestrian crosswalk at Central Avenue.

(east and west sides from Golden Avenue to North Avenue)

- Sidewalks adjacent to right-of-way on east side. Allow outdoor uses of public sidewalk that provide for a minimum of 5 feet of unobstructed sidewalk width.
- Enhanced pedestrian crosswalks at Broadway.

Lester Avenue

- Sidewalks adjacent to right-of-way on both sides.
- Enhanced pedestrian crosswalks at Lemon Grove Avenue and Grove Street, and one lighted mid-block crosswalk (could be raised).
- Provide bulb-out sidewalk to reduce street width at mid-block pedestrian crosswalk.

Golden Avenue

(north side from Lemon Grove Avenue to Kempf Street)

- Sidewalk adjacent to right-of-way on north side.
- Enhanced pedestrian crossing at Lemon Grove Avenue and raised pedestrian crossing at School Lane.

(south side from Lemon Grove Avenue to School Lane)

- Sidewalk adjacent to right-of-way on south side.
- Enhanced pedestrian crossing at Lemon Grove Street and raised pedestrian crossing at School Lane.

- Bulb-out sidewalk to reduce street width at pedestrian crosswalks.

Grove Street

- Sidewalk adjacent to right-of-way on west side. Allow outdoor uses of public sidewalk that provide for a minimum of 5 feet of unobstructed sidewalk width.

Kempf Street

- Sidewalks adjacent to right-of-way on west side. Allow outdoor uses of public sidewalk that provide for a minimum of 5 feet of unobstructed sidewalk width.

Main Street (east and west sides from Central Avenue to Pacific)

- Provide 20-foot wide sidewalk adjacent to right-of-way on west side. Allow outdoor uses of public sidewalk that provide for a minimum of 5 feet of unobstructed sidewalk width. No sidewalk on east side.

Main Street Promenade (east and west sides from Broadway to North Avenue)

- Development footprints (requires public road vacation process).
- Pedestrian pathways and plazas.
- Landscape enhancements.
- Access for Fire and Emergency and Maintenance vehicles.
- Incorporate shared bike and pedestrian paths with appropriate signage and bike racks.

Main Street (east and west sides from Specific Plan Boundary to Central Avenue)

- Provide 15-foot sidewalk adjacent to right-of-way on west side.

North Avenue

- Sidewalk adjacent to right-of-way on both sides.



Pacific Avenue

- Sidewalks adjacent to right-of-way on both north and south sides. Allow outdoor uses of public sidewalk that provide for a minimum of 5 feet of unobstructed sidewalk width.
- Raised or enhanced pedestrian walkways at Main Street.

Olive Street

- Sidewalk adjacent to right-of-way on east side.

Central Avenue

- Sidewalk adjacent to right-of-way on north side.
- Enhanced pedestrian crossing at Lemon Grove Avenue and raised pedestrian crossing at School Lane.
- Bulb-out sidewalk to reduce street width at pedestrian crosswalks.

School Lane

- Sidewalk adjacent to right-of-way on west side.

C.3 Bicycle Plan

Continue Implementation of the Lemon Grove Bikeway Master Plan Update adopted by the City Council on November 7, 2007. The design and construction of the Main Street Promenade and contiguous developments will incorporate shared bike and pedestrian paths with appropriate signage and bike racks.

D. Parking

An important issue to the revitalization of the Downtown Village Specific Plan is the provision of an adequate supply of parking for both commercial and residential development. At the same time, it is important to develop parking

standards that recognize the unique opportunity for transit-oriented development.

D.1 Existing Conditions

A parking study was prepared as part of the Lemon Grove Downtown Specific Plan process.

A parking inventory of available public-spaces (both curbside and lots) was conducted within the project area. Table 4 shows the existing parking supply within the project area for both curbside parking on public streets and public parking lots.

D.2 Proposed Parking Design Standards

Design Standards for Parking Spaces

- All parking spaces shall have a minimum size of 8.5 feet wide by 18 feet long. Add 6 inches to the width for each side of the stall that abuts a wall, column, or other immovable obstacle.
- Tandem spaces could be considered for parking lots with assigned employee parking or valet parking associated with a restaurant use.

Aisle Widths

- Two-way aisles for 90 degree parking shall be at least 24 feet wide, unless there is a greater requirement for fire lanes. Lesser widths are possible for angled parking and one-way aisles.

Driveway Widths

- Residential driveways should be between 16 and 30 feet wide (plus flares), with the greater width being for driveways with daily volumes expected to exceed 1000 vehicles. The greater width is also needed if there is no curbside parking and vehicles are entering the driveway from a position adjacent to the curb.

Table 4. Existing Parking Supply

| Street Name | Street Section | Side | Supply |
|--|-----------------------------|-------|------------|
| Lemon Grove Avenue | Lester Ave - Broadway | East | 7 |
| | Broadway – Golden Ave | East | 5 |
| Broadway | Lemon Grove Ave – Grove St | South | 64 |
| | Lemon Grove Ave – Grove St* | North | 66 |
| Main Street | Lemon Grove Ave – Broadway | East | 18 |
| | Lemon Grove Ave – Broadway | West | 34 |
| | Broadway – Central | East | 46 |
| | Broadway – Central | West | 31 |
| Lester Avenue | Lemon Grove Ave – Grove St | South | 21 |
| Pacific Avenue | Olive St – Main St | South | 9 |
| CURBSIDE TOTAL | | | 301 |
| Public Lot on Lester Avenue | | | 103 |
| Public Lot on Main Street (Library Lot) | | | 49 |
| Total Parking Supply | | | 453 |

* Located on private property

Commercial and office driveways shall be 30 feet wide minimum (plus flares). Signalized driveways may also be wider.

Street Parking

The Street Improvement Standards in section C.2 of this Specific Plan provide for the continuation of existing and creation of additional street parking spaces.

Angle parking will be considered on-street, depending on the street width and volume. Parking meters should be considered along Broadway, Main Street, and other streets to discourage employee parking and to encourage parking turnover.

Additional on-site parking will also be created for new or redevelopment projects. New parking areas shall be located and accessed as follows.

Village Core

- **Broadway & Lemon Grove Avenue -** Parking shall not be located fronting the street. It should be located in parking structures above grade or below ground or at grade in the rear of the lot behind the building. Access shall only be from the rear or side street.
- **Golden Avenue -** Parking may be located adjacent to Golden Avenue when buffered with landscaping a minimum of 4 feet in width.
- **Grove Street & Kempf Street -** Parking shall be located only in the rear of the lot behind the building. Access to parking behind building may be from taken from Grove Street.

Main Street Promenade

- **Broadway, Main Street and Pacific Avenue -** Parking shall be located in the rear of the lot or underneath the building. When feasible, access shall only be from Olive Street or North Avenue.
- **Olive Street and North Avenue -** Parking may be located adjacent to Olive and North Streets when buffered with landscaping a minimum of 4 feet in width.

Civic Center

- **Main Street -** Parking shall be located only in the rear of the lot behind the building. Access to parking may be from Main Street, Central Avenue or Olive Street.
- **Olive Street and Central Avenue -** Parking may be located adjacent to Olive Street and Central Avenue when buffered with landscaping a minimum of 4 feet in width.

Marketplace

- **Lemon Grove Avenue, School Lane, Central Avenue and Golden Avenue -** Parking may be located adjacent to the street



when buffered with landscaping a minimum of 4 feet in width.

- **Corner of Lemon Grove Avenue & Central Avenue** - Parking for the building at the corner of Lemon Grove Avenue and Central Avenue shall be located behind the building. Access to parking behind building shall be from Central Avenue.

E. Public Signage

E.1 Directional Signage

Visitors approaching the various districts within the Specific Plan area will need easily recognizable directional signs to help them locate important destinations. It is very important that these signs are graphically interesting, unique public signage and used in the commercial districts. It is recommended that all new Public Signage signs incorporate a designed logo that identifies the overall area of Downtown Lemon Grove.

With four Districts located in close proximity to each other, it is easy to over complicate the amount of directional signage required to effectively work within Downtown Lemon Grove. A two-layered system of directional signage should be considered: 1) Signage on State Route 94, 2) Signage that effectively guides motorists to parking destinations (and to the Civic Center District), and 3) signs that encourage pedestrian activity. The goal is to get drivers out of the car quickly upon arrival, thus reducing auto traffic and promoting pedestrian activity. Design criteria include those for automobiles, bicyclists, and for pedestrians.

The intent of auto directional signage should be to direct traffic to public parking lots once traffic enters the Downtown Village area to encourage pedestrian activity and reduce traffic on key corridors.

Automobile directional signage design criteria include:

- Directional signs for autos shall be placed at major District entry points and primary circulation centers such as intersections.
- Directional signage should be easy to locate, read and understand. Signs should be simple in design, and not visually compete with the surrounding context.
- Directional signage shall have easy to read letters using standard serif font set against a contrasting background.
- To simplify the amount of directional signs needed for autos, the following signs are recommended:
 1. Primary access routes to public parking lots
 2. Directional signage to the Civic Center & Public Safety Buildings
 3. Directional signage to the Village Core
- The design and materials for directional signs should support the overall character and identity of a “main street” downtown. However, signs shall be made of durable materials, easy to maintain and shall be vandal proof.

Pedestrian directional signage design criteria include:

- Directional signage should be easy to locate, read and understand. Signs should be simple in design, and not visually compete with the surrounding context.
- Pedestrian directional signs shall be placed at major District entry points and primary circulation centers such as plazas, paseo entries into a District, bus stops, Trolley Station, and public parks.
- Directional signage shall have easy to read letters using standard serif font set against a contrasting background.

- Pedestrian directional signs shall be human scaled, utilizing warm and inviting materials and design.
- The design and materials for directional signs should support the overall character and identity of a “main street” downtown. However, signs shall be made of durable materials, easy to maintain and shall be vandal proof.

E.2 Parking Signage

Parking signage directs users to parking areas and indicates the nearby activity. Working in conjunction with directional signage criteria for autos listed above, parking signage provides recognizable icons that quickly identify public parking lot locations.

- Parking signage shall be easy to locate, read and understand. Signs should be simple in design, and not visually compete with the surrounding context.
- Parking signage shall coordinate with directional signage in design, materials and colors.
- Parking signage shall have easy to read letters using standard serif font set against a contrasting background.

E.3 Street Signage

Street signage in the Downtown Village Specific Plan area should reinforce the area’s identity as being unique within the City of Lemon Grove.

- Street signage shall be easy to identify and read. Signs should be simple in design, and not visually compete with the surrounding context.
- Street signs shall coordinate with and overall signage theme.

- Street signs shall have easy to read letters using standard serif font set against a contrasting background.
- Map signs to orient visitors arriving at the future Integrated Transit Center shall be provided.

E.4 Identity Monument Signage

Identity monument signs within a community help establish a community’s “center,” strengthen its identity, and creates a landmark within Downtown.

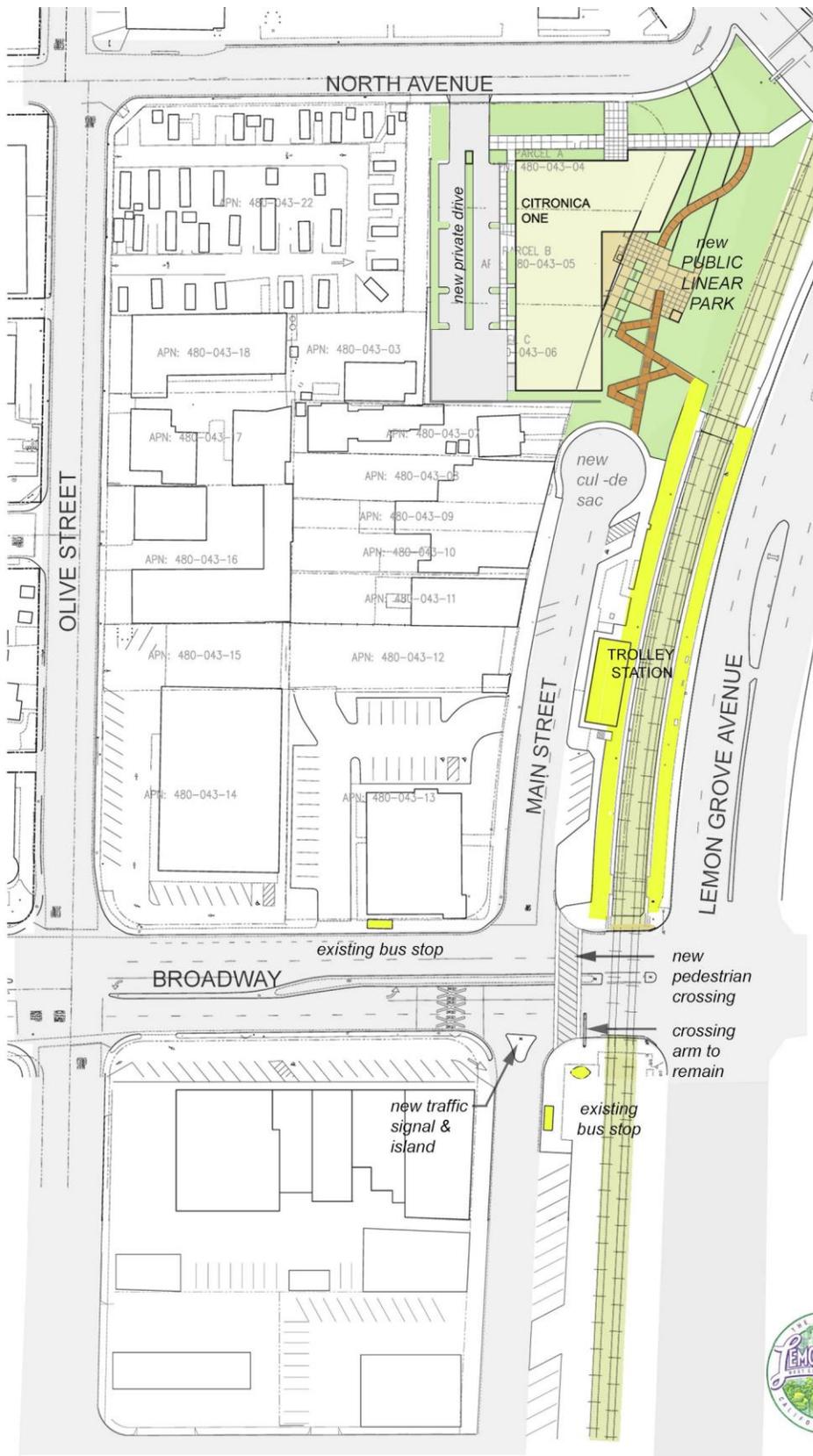
For Downtown Lemon Grove, it is recommended to locate a new identity monument sign at the existing center mid-block median at Broadway (see signage diagram). The plaza enhancement around the existing Lemon Monument will serve to emphasize the “heart” of the Downtown area.

The existing Lemon monument should remain in at this location, serving as a “historic marker” within the Main Street Promenade District of Lemon Grove.

Guidelines for future identity monument options are:

- Monument signage should be clearly visible both day and night from passing cars.
- Monument signs shall be accentuated with landscaping to distinguish it from secondary points of entry.
- The sign base shall be double-sided and should not have a height greater than 18-20’ or lower than 15’.
- Sign shall not encroach into the public right-of-way and shall be protected from vandalism and damage.





INTERIM MAIN STREET PROMENADE 



ULTIMATE MAIN STREET PROMENADE



CHAPTER V – INFRASTRUCTURE

A. Introduction

Utility service infrastructure such as water, wastewater, gas and electric facilities, and stormwater drainage facilities are essential services needed for the orderly growth and redevelopment of a community. Public safety services, including law enforcement and fire protection are also essential.

According to the City’s 1996 General Plan, service infrastructure is satisfactory to accommodate the General Plan land uses and intensity of use. The Specific Plan area is largely built out and though the Specific Plan increases the residential and commercial floor area, with the exception of a portion of the wastewater collection system, Lemon Grove’s existing infrastructure framework is considered adequate to service the Specific Plan land uses. For informational purposes, these facilities and public safety services are briefly discussed.

B. Water Service

The Helix Water District provides water service in Lemon Grove. About 90 percent of the District’s water supply originates in northern California or the Colorado River basin, and the remaining ten percent consists of local runoff within the San Diego region. The imported water is purchased from the San Diego County Water Authority, which obtains supplies from the Metropolitan Water District. Water is treated at the R.M. Levy Water Treatment Plant and distributed throughout Lemon Grove. The main components of the local system consist of the 30-million-gallon Grossmont storage tank, the 4-million-gallon Lemon Grove storage tank and a 27-inch transmission line located in Main Street.

Because the Downtown Village Specific Plan area and most of the City is almost completely developed, the District’s long-range plans primarily consist of maintaining the water infrastructure. Overall, the distribution lines in

Lemon Grove are in good condition and are adequate to meet the current and future needs of the City.

The capacity of localized portions of the Downtown Village Specific Plan system may require enlargement or replacement to accommodate new development or redevelopment that increases demand needs of the City. Water to fight fires typically requires the most infrastructure. Fire flows within the Downtown Village Specific Plan area are adequate and are currently in the range of 2,500 gallons per minute with 90 pounds per square inch of pressure. The City will require developers to coordinate with Helix Water District and identify necessary improvements. Helix Water District anticipates requiring the extension of approximately 700 feet of waterline to connect an existing water line in School Lane to the water line in Golden Avenue and adding about 500 feet of water line in Olive Street between Central Avenue and Madison Street. To ensure that facilities will not be oversized and induce undesired growth, the City will request that the capacity of new facilities reflect the development levels anticipated by the Downtown Village Specific Plan and General Plan.

C. Wastewater Service

The Specific Plan area lies entirely within the Lemon Grove Sanitation District, which provides sewer service within the City. The County of San Diego originally staffed and managed the district. When Lemon Grove incorporated in 1977, the City took over responsibility and is now managed by their Engineering Services Department. Sewage is collected and eventually directed to the Point Loma Wastewater Treatment Plant, operated by the City of San Diego Metropolitan Wastewater Department. A comprehensive engineering study was conducted in 1981 and anticipated a higher rate of growth than has been experienced in Lemon Grove, however, some portions of the system, such as trunk sewer lines, exhibit signs of deterioration and need replacement. Recent



blockages in the sewer trunk main between Massachusetts Avenue and the southern city limits have resulted in the discovery that a 4,000-foot section of 12-inch vitrified clay pipe constructed in 1947 has been severely damaged and is currently at capacity. Any significant increase in sewage generation within the Downtown Village Specific Plan will require the existing damaged trunk sewer main to be upsized to 18 inches.

For new development and redevelopment projects, the Lemon Grove Sanitation District, with assistance from the developer, will identify necessary localized infrastructure improvements needed to accommodate any increase in service demand.

Storm Drainage

The project area is located at the northern end of the South Chollas Valley. A tributary of Chollas Creek which trends in a southwesterly direction away from the site. There are no well-defined drainages, divides or ridges on the site, but surface water runoff is generally to the west and toward Lemon Avenue at the western boundary of the project area. Part of the City's existing storm drain system traverses the site from east to west. Redevelopment in existing urbanized areas does not typically increase the volume of stormwater runoff and require an increase in the size of drainage facilities. However, recent changes in stormwater quality standards will require new development or redevelopment to implement measures to improve the quality of stormwater runoff. Developers are required to provide a comprehensive drainage plan. The lowering of the San Diego Trolley will change stormwater runoff patterns and affect existing drainage facilities.

D. Power and Telecommunications Facilities

Electricity and natural gas is provided to the project area by the San Diego Gas & Electric Company (SDG&E). Electricity is transmitted by above ground and underground power lines from the Chollas Substation. Natural gas lines

exist along all major street rights-of-way within the area. SDG&E has indicated that existing gas and electric facilities have adequate capacity to serve the area and future densification. The lowering of the San Diego Trolley will affect existing underground power and gas lines, and will need to be addressed. Additional telecommunication and cellular telephone communications facilities are required to obtain a Conditional Use Permit within the Downtown Village Specific Plan.

E. Public Safety

Law Enforcement

Law enforcement services within Lemon Grove are provided by the San Diego County Sheriffs Department under contract with the City of Lemon Grove. The Sheriffs substation is located at 3240 Main Street, adjacent to City Hall in the Civic Center District. The response time to the area is 1 minute for priority calls. Priority calls are those that are life-threatening or those with the suspect on the scene. Increases in development intensity and residential density will increase the demand for law enforcement services.

Fire Protection

Fire protection within the Downtown Village Specific Plan area is provided by the Lemon Grove Fire Department (LGFD) located at 7853 Central Avenue. Staff includes two fully manned engine companies in one station. The LGFD has Automatic Aid agreements with the Cities of San Diego and La Mesa and the San Miguel Fire District. Mutual aid is also provided with the participation of all east county cities. The increased development intensity and height of structures in the Downtown Village Specific Plan may require the purchase of additional types of special equipment designed to operate around mid-rise structures.



CHAPTER VI – IMPLEMENTATION

A. Implementation Overview

The Downtown Village Specific Plan cannot be implemented solely through the actions and investment of public agencies. Typically, a high level of public involvement and investment is required in the beginning phases of a specific plan. This involvement includes public funding of capital improvement projects, providing incentives to private developers, adopting regulatory measures and policies that encourage desired development, and promoting the vision outlined in the Plan. However, these “seed” activities and investments act as a catalyst to stimulate private initiative and investment. Most importantly, the Plan’s implementation must involve active cooperation between property owners, business operators, residents, the City of Lemon Grove, and regional public agencies.

Long term sustainability and success of most downtowns also rely on the participation of property owners, business operators, investors and public agencies. To fully implement the Plan’s vision, there is a need for both private and public investment in new development, rehabilitation projects, the provision of adequate public and private parking, and the promotion of the Downtown area.

The Downtown Village Specific Plan must be adopted and amended in the same method as a General Plan, although it may be adopted by resolution or by ordinance and may be amended as often as deemed appropriate by the City Council. Once the Plan is adopted, local public works projects, tentative maps, final or parcel maps, zoning and new ordinances require consistency with the Plan. Also, as with the General Plan, it is the responsibility of the City to ensure the implementation of the Plan.

This chapter addresses strategies to implement elements identified in the Downtown Village Specific Plan. Particularly, this chapter:

1. Prioritizes capital improvement projects that will support the vision presented in the Plan,

2. Recommends programs that can be offered or initiated by the City of Lemon Grove, the Lemon Grove Community Development Agency (Agency), and the business community to aid in facilitating the accomplishments of goals presented in the Plan,
3. Identifies financing sources to implement the Plan, and
4. Addresses administration of the Plan.

B. Projects & Programs

The Downtown Specific Plan encourages the development and revitalization of the downtown area. There are several critical capital improvement projects that are required to either facilitate and/or accommodate the Plan’s objectives. This section describes key capital improvement projects and possible programs that can be implemented.

Public Capital Improvements

1. Mid-block Crosswalk – To facilitate effective pedestrian circulation and to promote walkability within the downtown area, there is a need for a mid-block crosswalk on Broadway. For maximum benefit, the crosswalk should be closely aligned with the Lester parking lot breezeway. The crosswalk can be initiated by the City using transportation grant funds.
2. San Diego Trolley Issues (Grade Separation and Coordinating Trolley with Traffic Signals) – Currently, the San Diego Trolley traverses the downtown. The trolley tracks pose connectivity challenges to the community as identified in the Plan. As such, the Plan calls for grade separation of the trolley line. However, the cost of grade separation (whether elevating or lowering the tracks) is significant. Until a full engineering study is conducted, it is difficult to provide an accu-



rate cost; however, an estimate is between \$35 million and \$45 million.

In the interim period, the Plan calls for an interim solution for congestion caused by the trolley. The Plan recommends that trolley movement through the Broadway/ Lemon Grove Avenue intersection and the grade crossing at Lemon Grove Avenue be controlled by traffic signals.

Both the interim and long-term solutions require coordination with MTS and eventually action by MTS. It is recommended that representatives from the City, MTS and SANDAG meet to discuss solution for MTS to implement.

Grade Separation will require the planning and financial participation from many funding sources – SANDAG smart growth and CMAQ, redevelopment, private developer, and General Fund. A grade separation project's scope is complex, requires substantial financial commitment, and relies on the cooperation of several public agencies. As such, it is unlikely that achieving the goal of grade separation will occur in the near future. Due to the significant level of financial commitment from the City, it is an item that would require further City deliberation and planning.

3. Entryway and Directional Signage – The City, through its CIP, should develop a schedule and plan to install entryway signage as specified in the Plan. In addition, the City should require the installation of directional signage by the developers of larger projects in the Plan area (i.e. development comprising more than one acre).
4. Median Improvements – Lower planter pyramids located in the Broadway median and replant street trees.
5. Consolidation of Bus and Trolley Stops – At Plan adoption, the MTS operated bus system stop and the San Diego Trolley platform are not co-located. The two facilities are located on opposite sides of Broadway. As such, challenges exist for those using public

transit and traffic circulation is negatively impacted. The Plan recommends that staff from SANDAG, MTS and the City of Lemon Grove develop a strategy to co-locate the bus and trolley stops.

Public/Private Capital Improvements

1. Parking Facilities – Downtown parking seems to be a challenge in many communities. Typically, downtown parking infrastructure does not meet the demands of municipal parking policy requirements. This challenge is currently experienced in Lemon Grove's downtown area as well. However, there are opportunities to create additional public and private parking. The Plan identifies areas in which parking lots and structures can be developed. Primarily, three likely parking projects include:

- A parking lot or structure located on privately owned property at Broadway and Golden Avenue.
- An expansion of the Lester Avenue parking lot to the east, necessitating the relocation of the U.S. Post Office's distribution operation to a more suitable location.
- A parking structure located at the existing publicly owned Lester Avenue parking lot.

The development of these facilities will rely on public funding, private funding, or both. Three financing tools for the development of these facilities include:

- Creating a parking space in-lieu fee for all new uses in a specific business district that do not meet parking requirements.
- Imposing an assessment on property owners to fund long-term debt for the development of additional parking facilities.



- Utilizing redevelopment funds to initiate the development of parking facilities.
2. Sidewalk, Curb and Gutter – An effective sidewalk system includes maintained sidewalks that are installed where pedestrians have need for them. A walking tour of the downtown revealed that installation of additional sidewalks would enhance the sidewalk system. Specifically, sidewalks should be installed at:
 - North and south sides of Lester Avenue,
 - North side of Golden Avenue,
 - Northeast corner of Main Street and Broadway, and
 - Other areas of the downtown require sidewalk improvements and walkability enhancements (security from traffic, comfort, welcoming features, and efficient use of space).

The sidewalk, curb and gutter gaps on the streets identified above should be required to be installed by property owners that trigger the centerline ordinance. The City may also consider initiating the improvements after obtaining the appropriate right-of-way.

3. Landscape/Open Space – The intention of the Plan is to maximize development in the Plan area. The aim reduces the ability to allocate sufficient open space to meet the needs of potential downtown residents. As such it is important to create pockets of open space in the larger development projects, maximize landscaping opportunities, and create open space and outdoor recreational opportunities in the areas surrounding the Plan area.

It is recommended that the City institute a park in lieu fee that can be used to purchase land for recreational purposes. These land purchases can occur adjacent to existing park open space areas or in new areas. It is also recommended that both large and small

development contribute funds for landscaping enhancements. Landscaping improvements and maintenance are eligible projects and programs for business improvement and assessment districts.

4. Sewer Trunk Capacity Enhancement – In preparing the Plan, it was determined that the sewer trunk that serves the Plan area does not have the capacity to accommodate the Plan’s built out flow. It is recommended that when larger projects are proposed, that developers are required to determine the project’s impact on the sewer trunk. It is also recommended that the Sanitation District include in its CIP a plan to widen the line funded in part by development fees associated with new development in the Plan area.
5. Storm Drain Improvements – There is no comprehensive storm drain system in the Plan area. There is need for storm drain improvements on North Street and Grove Street. It is recommended that when larger projects are proposed, that developers are required to prepare specific storm drain plans for that area. When funding is available, the City should consider a comprehensive storm drain plan for the Plan area.

Programs

1. Business Grant and Façade Rehabilitation Programs – Many cities and redevelopment agencies throughout California have implemented successful façade rehabilitation programs and small business grant programs aimed at giving aging commercial corridors a face life and promoting economic development. Typically, the programs require matching funds from the property or business owner to complement the public funding. Programs also require compliance with standards established by the local municipality.
2. Downtown Promotions Program – A comprehensive promotion program could assist with increasing consumer awareness of businesses in the downtown area. This



program could be privately or publicly funded. However, when publicly funded it is typical for a non-profit, chamber of commerce or business district association to organize and manage the promotional activities.

3. Downtown Business Association – One of the most successful means to creating an energetic downtown is through the creation of a grassroots downtown business association. An association could promote commercial opportunities and advocate on behalf of the merchants’ interests in the district. A business association could also spearhead the creation of a business improvement district that could finance capital improvements that would benefit the entire business district.
4. Affordable Housing Assistance Program – One of the objectives of the Lemon Grove Community Development Agency is to facilitate the increased availability of affordable housing. The Plan calls for mixed-use development that includes a critical housing component. The Agency could create a program in which financial incentives would be offered to developers in exchange for the development of affordable housing units. Other incentives, such as density bonuses and assistance with infrastructure improvements (including parking), could also be included in the program.

Financing Sources

A variety of financing tools will be required to fully implement the Plan’s goals and objectives. This section identifies possible funding and financing sources to realize the Plan’s vision.

1. General Obligation Bonds – Projects that benefit an entire jurisdiction may be financed through General Obligation Bonds. Only improvements that are general in nature, such as community facilities (street and drainage improvements) would be eligible projects. General Obligation Bonds require approval of the jurisdiction’s voters.

2. Lighting and Landscaping District – Lighting and landscaping districts are formed to finance:

- Lighting and landscaping provided by the City,
- Land acquisition for parks and open space, and
- Expenses associated with the formation of the District.

A district is initiated by the City and funded by assessments of those within the district’s boundaries. Formation of a district requires the approval of property owners within its borders.

3. Special Assessment District – Special assessment districts are formed to finance physical improvements that are shared by property owners within an area. Examples of projects include parking lots, installation of curb/gutter/sidewalk, and extension of sewer line.

Special districts are funded from assessments levied on property owners to pay back the initial cost of installing the improvement. Assessments vary on the level of benefit each property owner derives from the improvement as determined by an Engineer’s Report. Formation of a district requires the approval of property owners within the district’s borders.

4. Business Improvement District – Business improvement districts can fund a wide range of projects and programs. Eligible activities that can be supported by business improvement districts include:

- Facade improvements
- Parking facilities
- Street lighting
- Promotional activities



- Business Association (staff, office space, equipment, etc.)

Business improvement districts are funded through an assessment levied on either the property owners or business owners. District formation can be initiated by the City or by the owners of businesses or property. The formation of a district would require approval by the City and by at least 50 percent of those to be assessed.

5. Small Business Administration 504 Loan Program – The SBA 504 Loan Program privately finances projects such as acquisition, construction, and equipment purchases by small businesses. Funding is from long-term fixed rate second mortgages on the property.
6. Transportation Funds – there are a variety of transportation specific grant programs
 - Congestion Mitigation and Air Quality (CMAQ) – a Federal government funding source for transportation projects that reduce air pollution (i.e. installing curb and gutters, implementing signal interconnects, reducing traffic congestion).
 - Transportation Equity Act 21 – a Federal government funding source aimed at improving highways, highway safety, and transit programs.
 - TransNet – a San Diego County regional sales tax allocation for improving regional transportation systems.
 - Surface Transportation Program – a Federal government funding source to be used on public road and transit capital projects.
7. SANDAG Programs
 - Planning Grants – SANDAG offers a Local Technical Assistance grant to jurisdiction for planning studies.

- Smart Growth Funds – through the 2008 TransNet program, SANDAG plans to offer grants to jurisdictions for implementing smart growth development.

8. Lemon Grove Community Development Agency – The Lemon Grove Community Development Agency (Agency) is a redevelopment agency authorized with a stated purpose to:

- Eliminate blight and blighting conditions in the project area,
- Encourage business attraction, expansion, and retention in the project area, and
- Promote affordable housing opportunities in the City.

Redevelopment funds can be utilized in a myriad of ways ranging from financing capital improvements, acquiring land, offering grants and loans to businesses and developers, and supporting activities that assist in the accomplishments of the Agency’s Five Year Implementation Plan.

9. HOME – a Federal government funding source to promote affordable housing.
10. City of Lemon Grove General Fund – the City’s General Fund has the flexibility to initiate programs and projects that are deemed in the best interest of the City. However, because of its flexibility, it is the primary funding source for basic municipal services—public safety, parks, recreation, administration, and community development.
11. Revenue Bonds – Governmental agencies are authorized to sell revenue bonds that are secured with a specific revenue source as a result of a specific project. However, the types of public facilities anticipated to be constructed in the Plan area are not revenue generating projects. The only opportunity to



use this funding source is if parking structures or lots were financed through user fees.

12. Impact Fees – Local agencies, under California law, may impose an impact or developer fee to finance public improvements that mitigate the impact of development. For example, the Lemon Grove Sanitation District collects a development fee from new developers to finance the expansion of the sewer system to accommodate sewage flow from the new development. Similarly, the City could impose impact fees related to parking, traffic signals, public infrastructure, open space/parks on new development projects.

Environmental Compliance

Projects within the Downtown Specific Plan Area will be reviewed on an individual basis. Additional air quality, cultural resources, noise, geology and soils, hazards and hazardous materials, public services and utilities, and traffic analyses will be required where deemed appropriate in relationship to the Final Mitigated Negative Declaration and the General Plan Master EIR.

